



North Carolina Department of Cultural Resources
State Historic Preservation Office

Ramona M. Bartos, Administrator

Beverly Eaves Perdue, Governor
Linda A. Carlisle, Secretary
Jeffrey J. Crow, Deputy Secretary

Office of Archives and History
Division of Historical Resources
David Brook, Director

August 7, 2012

MEMORANDUM

TO: Shelby Spillers
Office of Human Environment
NCDOT Division of Highways

FROM: Ramona M. Bartos

RMB for Ramona M. Bartos

SUBJECT: Intensive-Level Historic Architectural Analysis, Replace Bridges 227 and 213 on US 70 (Capital Boulevard) over Peace Street and Wade Avenue, B-5121 and B-5317, Raleigh, Wake County, ER 08-2607 and ER 11-0310

Thank you for your letter of July 3, 2012, concerning the above project. We appreciate NCDOT's efforts to supplement the architectural resources survey prepared by Dovetail Cultural Resource Group with additional context for the Gulf Gas Station and by providing information on a number of comparable examples of this property type.

We agree with your list of common elements of the oblong box design, although we believe that the retention of the overall form (including a canopy if original), construction and materials, garage bay doors, and the storefront and personnel doors are of greatest importance. In general, a gas station of this type that is individually eligible for listing in the National Register of Historic Places under Criterion C will possess all or most of these features. Although North Carolina has not developed formal eligibility requirements for gas stations, W. Dwayne Jones offers advice in his 2003 report "A Field Guide to Gas Stations in Texas:"

For practical consideration, an individually eligible station should retain form, architectural style or influence, as well as principal spaces used to fuel or service automobiles (display area, service bays, service drives, canopy, etc.).¹

We believe that the loss of the original garage bays doors and storefront door and their modern, incompatible replacement doors, is a major strike against the building's architectural integrity. The loss of interior features, gas pumps and the pump island(s), and lighting further diminish the integrity of materials and design.

The study of seven comparable gas stations is quite helpful in that it shows that although most sites retain some of the important elements of the oblong box form, none retain all. However, it appears that several of the six Raleigh gas stations examined retain a higher level of integrity than the Capital Boulevard Gulf.

¹ Jones, W. Dwayne. *Historical Studies Report No. 2003-03: A Field Guide to Gas Stations in Texas*. Texas Department of Transportation, 2003.

Although 600 West Peace Street and 2001 Wake Forest Road each lack a canopy, it is unclear at this time whether their original design included a canopy. Otherwise, these two stations each retain their form, construction, materials, storefront, and garage doors. We also note that 501 West Morgan, which appears to have comparable integrity, was recently surveyed by Mattson Alexander and Associates and found by NCDOT and SHPO to be not worthy of an in-depth determination of its National Register eligibility. Given the Gulf Gas Stations relative lack of integrity, we continue to believe that it is not eligible for listing in the National Register under Criterion C.

In your letter, you also suggest that the Gulf Gas Station is eligible under Criterion A for roadside commerce and branding. The Gulf does retain the three “Gulf bands” (now repainted) above the garage and storefront, but the station does not appear to have any other distinctive branding elements. The identifying features of 1940-70 Gulf stations noted by Jones, including a flat roof, display windows, multi-light garage doors, and porcelain enamel panels, are common to many stations of this era.²

While the station opened shortly after the opening of Capital Boulevard, the Wake County Real Estate records show that the adjacent property at 909 Capital Boulevard was historically a circa 1955 Philips 66 gas station. This property has now been altered beyond recognition, but the property description (909 DOWNTOWN BLVD PHILLIPS 66 SER STA) and 1995 tax assessment photograph clearly show that the Gulf Station was not the only nor the first service station to operate on this stretch of Capital Boulevard.³ At this time, we do not concur that the Gulf Gas Station is historically significant for its association with roadside commerce.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation’s Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579. In all future communication concerning this project, please cite the above-referenced tracking number.

cc: Martha Hobbs Lauer, Raleigh Historic Development Commission, mhobbs@rhdc.org
Vince Rhea, NCDOT, vrhea@ncdot.gov
Felix Davila, FHWA, Felix.Davila@dot.gov

² Ibid, 69 and 87.

³ *Wake County Real Estate Data*. n.d. <http://services.wakegov.com/realestate/Account.asp?id=0071541> (accessed July 2012).



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Ramona M. Bartos, Administrator

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Division of Historical Resources
David Brook, Director

May 9, 2012

MEMORANDUM

TO: Mary Pope Furr
Office of Human Environment
NCDOT Division of Highways

FROM: Ramona M. Bartos *RMB for Ramona M. Bartos*

SUBJECT: Intensive-Level Historic Architectural Analysis, Replace Bridges 227 and 213 on US 70 (Capital Boulevard) over Peace Street and Wade Avenue, B-5121 and B-5317, Raleigh, Wake County, ER 08-2607 and ER 11-0310

On April 17, 2012, we received the North Carolina Department of Transportation's historic architectural resources survey report for the above project, prepared by Dovetail Cultural Resources Group.

For the purposes of compliance with Section 106 of the National Historic Preservation Act, we concur that the following properties are eligible for listing in the National Register of Historic Places under the criteria cited, and that the proposed National Register boundaries appear appropriate:

- ◆ **Raleigh Cotton Mill** (WA 3919, Inventory #19), Criterion A for industry and manufacturing, and Criterion C for architecture;
- ◆ **Noland Plumbing Company Building** (WA 7126, Inventory #7), Criterion A for commerce and industry, and Criterion C for architecture; and
- ◆ **Seaboard Air Line Turntable and Raleigh Gaston Railroad Roundhouse Site** (WA 7383, Inventory #22) as a contributing resource within the Raleigh and Gaston Railroad Corridor.

At this time we are unable to concur with your finding regarding the **Gulf Gas Station** (WA 7363, Inventory #13). While this station does retain its integrity of location, setting, feeling, and association, the loss of the original bay doors, entrance doors, lighting fixtures, and gas pumps have compromised the integrity of design, materials, and workmanship.

The cited modernist context study by Ruth Little only covers resources built after 1945, but the oblong box form of service station were first introduced in the 1930s, meaning a number of comparable stations were outside the scope of the context study.¹ The most notable example being Gibbon's Esso Service Station (WA 2887, built circa 1935) at 623 West Hillsborough Street, which was determined eligible in 1999. Other examples of this form, including several with the same enameled panels, are found throughout Raleigh, such as: the

¹ John A. Jakle and Keith A. Sculle, *The Gas Station in America* (Baltimore: The Johns Hopkins University Press, 1994), 144-152.

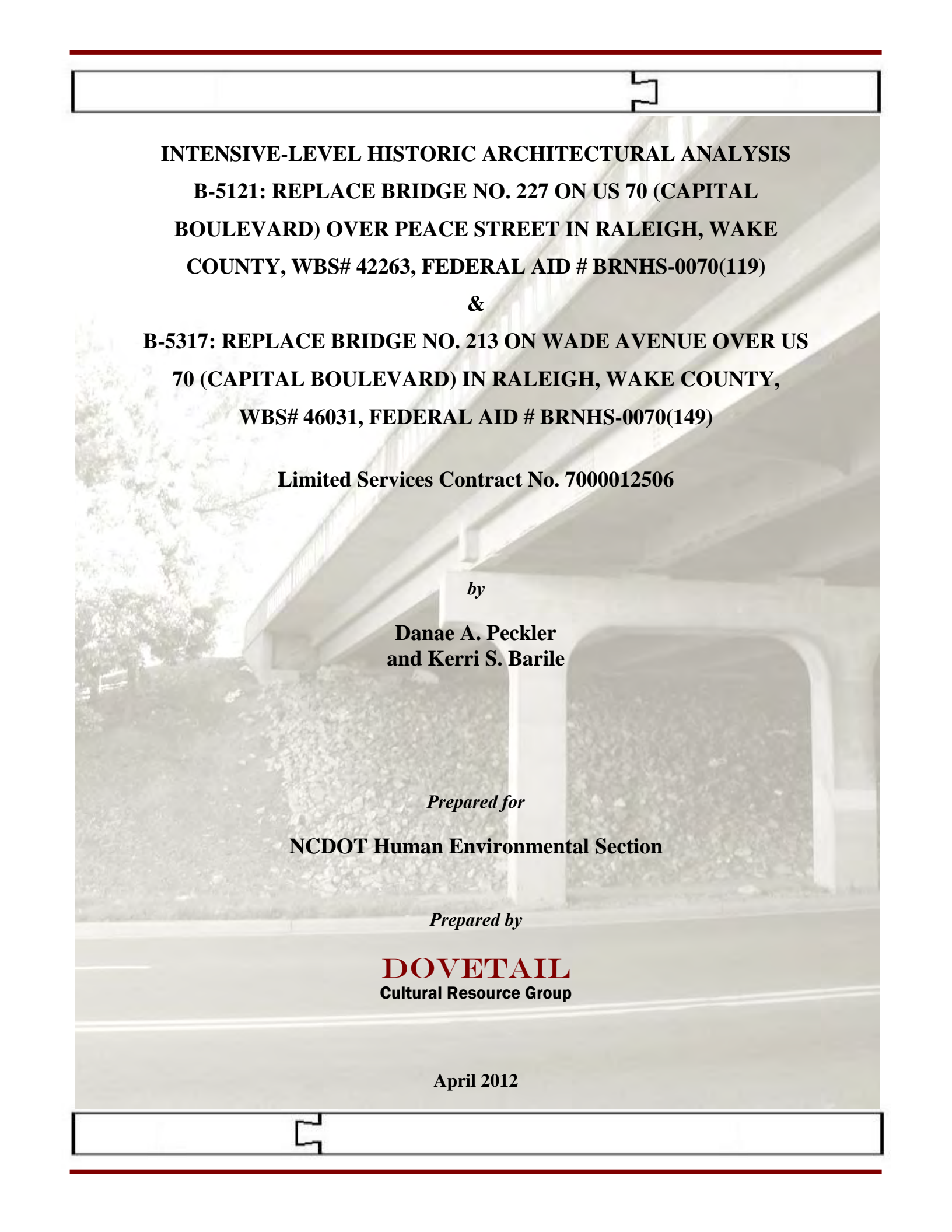
Wilmont Service Station (WA 2902, built circa 1940) at 3120 Hillsborough Street, the former service station at 501 West Morgan (WA 3008, built circa 1930), the service station at 630 Hillsborough Street (built circa 1952), the former service station at 600 West Peace Street (unknown date), and the service station at 2001 Wake Forest Road (built circa 1940). All of these appear to be more comparable to the Gulf Gas Station than the service station at 2121 Garner Road that was included in the context study. Since this was a very common form we have not typically found service stations of this type and era individually eligible unless the site is unaltered or only minimally altered. We therefore do not believe that the Gulf Service Station is eligible at this time.

We concur with your findings that, barring additional information to the contrary, **Finch's Restaurant** (WA 4607, Inventory #34) and the remaining 35 properties included in the **Appendix** are not eligible for listing in the National Register of Historic Places.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579. In all future communication concerning this project, please cite the above-referenced tracking number.

cc: Martha Hobbs Lauer, Raleigh Historic Development Commission, mhobbs@rhdc.org



**INTENSIVE-LEVEL HISTORIC ARCHITECTURAL ANALYSIS
B-5121: REPLACE BRIDGE NO. 227 ON US 70 (CAPITAL
BOULEVARD) OVER PEACE STREET IN RALEIGH, WAKE
COUNTY, WBS# 42263, FEDERAL AID # BRNHS-0070(119)**

&

**B-5317: REPLACE BRIDGE NO. 213 ON WADE AVENUE OVER US
70 (CAPITAL BOULEVARD) IN RALEIGH, WAKE COUNTY,
WBS# 46031, FEDERAL AID # BRNHS-0070(149)**

Limited Services Contract No. 7000012506

by

**Danae A. Peckler
and Kerri S. Barile**

Prepared for

NCDOT Human Environmental Section

Prepared by

DOVETAIL
Cultural Resource Group

April 2012

**Intensive-Level Historic Architectural Analysis for
B-5121: Replace Bridge No. 227 on US 70 (Capital Boulevard) over
Peace Street in Raleigh, Wake County, WBS# 42263, Federal Aid #
BRNHS-0070(119)**

&

**B-5317: Replace Bridge No. 213 on Wade Avenue over US 70 (Capital
Boulevard) in Raleigh, Wake County, WBS# 46031, Federal Aid #
BRNHS-0070(149)**

Limited Services Contract No. 7000012506

Prepared for

North Carolina Department of Transportation

Human Environment Section

1598 Mail Service Center

Raleigh, North Carolina 27699-1598

Prepared by

Danae A. Peckler and Kerri S. Barile

Dovetail Cultural Resource Group

300 Central Road, Suite 200

Fredericksburg, Virginia 22401

(540) 899-9170

Dovetail Job #11-033 & #11-044

April 2012

Principal Investigator
Dovetail Cultural Resource Group I, Inc.

Date

Supervisor, Historic Architecture Group
North Carolina Department of Transportation

Date

MANAGEMENT SUMMARY

Dovetail Cultural Resource Group I, Inc. (Dovetail) conducted historic architectural surveys on behalf of the North Carolina Department of Transportation (NCDOT) in preparation for the replacement of Bridge No. 227 (Capital Boulevard) over W. Peace Street, and Bridge No. 213 (Wade Avenue) over Capital Boulevard in the City of Raleigh, Wake County, North Carolina. The North Carolina Project Numbers are B-5121, WBS# 42263, Federal Aid# BRNHS-0070(119), and B-5317, WBS# 46031, Federal Aid# BRNHS-0070(149), respectively.

The current investigation included a building inventory, conducted in October 2011, which identified 40 properties over 50 years of age within the combined Area of Potential Effects (APE) for both projects. Five of the 40 resources were selected for further study following a review of inventoried properties by the North Carolina Historic Preservation Office (HPO) on November 15, 2011. At the request of NCDOT, an intensive-level survey of the five selected properties was conducted in January 2012 to gather additional data and assess the eligibility of these resources for the National Register of Historic Places (NRHP).

Two of the five historic resources selected for intensive survey had been previously determined eligible for the NRHP by the HPO: Raleigh Cotton Mill (WA 3919) and Noland Plumbing Company Building (WA 7126). Another selected resource had been previously surveyed, but not yet evaluated for the NRHP, Finch’s Restaurant (WA 4607). The remaining two properties, Gulf Gas Station (WA 7363) and Seaboard Air Line Railroad Turntable and Roundhouse Site (WA 7383), were newly recorded with the HPO as a result of this undertaking. Based on additional field and archival research, Dovetail makes the following recommendations regarding the NRHP-eligibility of these five historic resources:

Table 1: Eligibility Recommendations.

Raleigh Cotton Mill	WA 3919	Remains Eligible for the NRHP under Criteria A and C.
Noland Plumbing Company Building	WA 7126	Remains Eligible for the NRHP under Criteria A and C.
Gulf Gas Station	WA 7363	Recommended Eligible for the NRHP under Criterion C.
Finch’s Restaurant	WA 4607	Recommended Not Eligible for NRHP under Criteria A–D.
Seaboard Air Line Turntable and Raleigh & Gaston Railroad (RGRR) Roundhouse Site	WA 7383; 31WA1448	Recommended Not Eligible under Criteria A–C for NRHP as an individual architectural resource, but is considered a contributing element to the NRHP-Eligible RGRR corridor. This resource has been previously determined Eligible for the NRHP as an archaeological resource (31WA1448).

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INTRODUCTION

Dovetail Cultural Resource (Group) completed a historic architectural investigation on behalf of the Human Environment Unit of the North Carolina Department of Transportation (NCDOT), in preparation for two bridge replacement projects in the City of Raleigh, Wake County, North Carolina. The projects are known as Replacement of Bridge No. 227 (Capital Boulevard) over W. Peace Street and Replacement of Bridge No. 213 (Wade Avenue) over Capital Boulevard. The project numbers associated with these undertakings are B-5121, WBS# 42263, Federal Aid# BRNHS-0070(119), and B-5317, WBS# 46031, Federal Aid# BRNHS-0070(149), respectively.

Both bridge replacement projects are located within the Capital Boulevard corridor just north of Raleigh's downtown grid. In addition to replacing the bridges, project work will also require modifications to the surrounding ramps and service roadways. The combined Area of Potential Effects (APE) for these projects includes the project footprint and all areas where alterations to an historic resource's setting and feeling could occur, specifically the space between, and surrounding, the bridges, as well as affected ramps and service roadways. The APE extends roughly 1 mile along both sides of Capital Boulevard, beginning at a point just north of the Fairview Road bridge and continuing south to the railroad overpass, just prior to the boulevard's division into Dawson and McDowell Street (see Figure 1 and Figure 2, p. 2; Figure 3, p. 3).

Dovetail staff Danae Peckler, Heather Dollins, and Dr. Kerri Barile conducted the work for this undertaking, with Dr. Barile serving as Principal Investigator and Ms. Peckler acting as Project Manager. All three individuals meet or exceed the standards established for both Architectural Historian and Historian by the Secretary of the Interior (SOI). Work on this project was conducted between October 2011 and January 2012 in accordance with relevant state and federal guidelines as part of the compliance process established in Section 106 of the National Historic Preservation Act of 1966, as amended (36 C.F.R. 800).

This investigation included a building inventory of historic properties within the combined APE, and an intensive-level survey of five properties that merited additional study prior to an evaluation of eligibility for listing on the National Register of Historic Places (NRHP). Results of the building inventory are detailed in Appendix A, while the results of the intensive investigation and NHRP-eligibility recommendations for the select resources are presented within the body of this report. The report appendix includes the building inventory table and corresponding maps, a copy of the Concurrence Form noting properties determined not eligible on November 15, 2011, as well as brief descriptions, photographs, and evaluations of all historic resources identified as a result of this undertaking. This report meets the standards set forth by the HPO's Architectural Survey Manual, *Practical Advice for Recording Historic Resources*, as well as those outlined by NCDOT in *Sections 106 Procedures and Report Guidelines*.

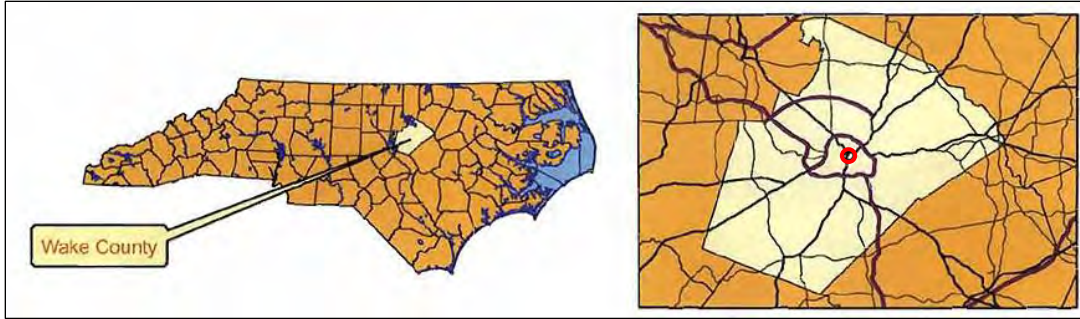


Figure 1: Map of Wake County in the State of North Carolina, with General Location of Project Area in the City of Raleigh Highlighted in Red (NCDOT 2011).

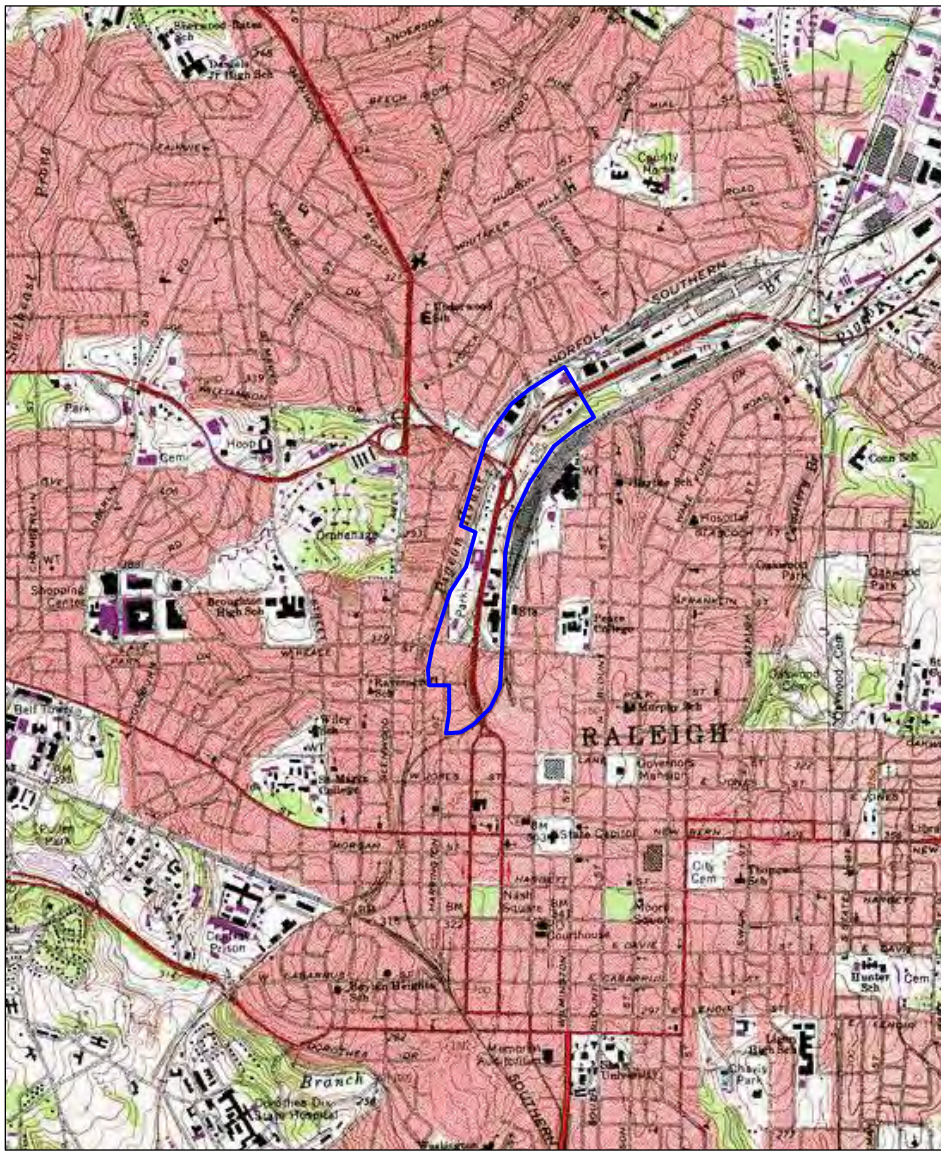


Figure 2: General Overview of Project Vicinity with APE Outlined in Blue over 7.5-Minute Raleigh West Topographic Quadrangle (United States Geographic Survey [USGS] 1999).

PROJECT METHODOLOGY

The project complied with Section 106 standards for the identification of historic properties. The goals of the survey were: one, to identify architectural resources over 50 years in age within the APE —as well as more recently constructed resources of exceptional significance; two, to document the historic properties through field notes and digital photography; three, to assess the historic integrity of each resource; and four, to present these findings at a building inventory review meeting to officials at NCDOT and the HPO. Following a review of HPO records, Dovetail conducted a survey of the project APE in October 2011.

During the survey, Dovetail staff visibly inspected the APE and recorded 100 percent of the existing historic resources; no above-ground resources were found that met the NRHP's additional criteria considerations (A–G). Once identified, each historic resource was documented through photographs, written field notes, and GIS mapping. Digital photographs were taken of each surveyed property's general setting, primary elevation, oblique angles, and significant architectural details. Inventoried resources were assigned a number corresponding to the building inventory and keyed to a map of the project APE. The building inventory, corresponding maps, brief descriptions, and photographs of the 40 historic properties are presented in Appendix A.

On November 15, 2011, NCDOT and HPO staff reviewed the building inventory, requesting that five historic resources be subjected to further review and evaluation. This list included two properties for which a previous “determination of eligibility” (DOE) had been made and three others: Finch’s Restaurant (WA 4607), Gulf Gas Station (WA 7363), and the Seaboard Air Line Railroad Turntable and Roundhouse Site (WA 7383). The goals of the intensive survey were: one, to gather additional historic and physical data for select historic properties to document architectural details, construction methodology, and chronology of each resource; two, to examine the history of each property within the appropriate context; three, to make recommendations regarding eligibility under established criteria for the NRHP; and four, to propose NRHP boundaries for any property that is recommended eligible for listing.

In January 2012, Dovetail visited several repositories in Raleigh to examine records regarding the city’s historical development and gather specific information on the history of each selected resource. These repositories included the Olivia Raney Local History Library, the Wilson Library at the University of North Carolina (UNC), and the State Library of North Carolina, more specifically the Government and History Research Room, State Archives, and Genealogy Research Rooms. Primary sources like City directories, local newspapers, promotional pamphlets, photographs and historic maps were examined. Numerous secondary sources detailing Raleigh’s history were also consulted, and relevant records at the HPO’s Survey and National Register Branch, the Office of State Archaeology were reviewed. Additional sources were retrieved online, including deeds, plats, and various maps, from various government sources like the Wake County Register of Deeds, North Carolina Maps Collection, Library of Congress, and National Park Service websites.

Once archival research was completed, Dovetail conducted an intensive-level architectural survey of the selected properties. Each property was visually inspected, recording exterior and interior spaces through written notes and digital photographs. Site plans of each property, showing the location of all buildings and landscape elements were created, as well as interior plan drawings, wherever possible. The historic development, architecture, and cultural significance of each resource were then assessed and evaluated within their respective contexts according to the Criteria established for the NRHP. The results of this intensive-level investigation and NRHP evaluation are presented in following chapter of this report.

RESULTS OF ARCHITECTURAL INVESTIGATION

The majority of the historic resources identified during the survey are mid-twentieth century commercial and industrial properties (n=32). Five other resources stem from twentieth-century transportation trends including four bridges, three for automotive use and one for rail. Other transportation-related resources include a culvert to channel Pigeon House Branch under W. Peace Street, and the SAL railroad turnaround and RGRR roundhouse site. Just one early-twentieth-century residential property was identified during the reconnaissance survey, but it has been adapted for commercial use.

Resources Previously Determined Eligible for the NRHP

The two historic properties determined eligible for the NRHP (Raleigh Cotton Mill and Noland Plumbing Company Building) have been surveyed, researched, and evaluated during previous investigations. Therefore, the history and physical documentation for these properties was less extensive than the research and analysis of the other three resources.

WA 3919: Raleigh Cotton Mill



614 Capital Boulevard
Raleigh, North Carolina
PIN: 1704622417

Previous Evaluations

The Raleigh Cotton Mill, currently addressed at 614 Capital Boulevard, was first added to the state's Study List in 1975 following a Historic American Engineering Record (HAER) survey conducted by Brent D. Glass. In the mid-1990s, developers bought the property and sought NRHP status in effort to convert the building into condominiums. Using historic tax credits, the property was rehabilitated in 1996 and later determined to be a local historic landmark (Raleigh Historic Development Commission). In a letter dated June 30, 1999, the

HPO first determined the property eligible for the NRHP, agreeing with a consultant's recommendation that the building met the requirements of Criterion A for the significant role it played in Raleigh's industrial development during the late-nineteenth century. This eligibility assessment was upheld following a May 2005 investigation, but additional information provided in Mattson, Alexander, and Associates, Inc.'s (MAA) Phase II report also highlighted the building's significance under Criterion C. Crediting mill architect C. R. Makepeace of Providence, Rhode Island with its design, this report detailed the significance of the Raleigh Cotton Mill's architecture and its place within the City's "small-scale" textile industry:

Constructed in 1890, the mill illustrates the rise of the textile industry in Raleigh and the region. Though Raleigh was never a major manufacturing city, the years around the turn of the twentieth century saw the emergence of small-scale textile factories beside the railroad tracks.... The mill's thick brick walls, bracketed gable roof, banks of arched windows, and heavy wood beams and rafters typifies the small-scale textile mills of the period. Such features illustrate slow burning mill construction that distinguished textile plants of this era (MAA 2005:46).

Physical Description

In a 1996 local Landmark Designation Application, Langdon Oppermann describes the Raleigh Cotton Mills architecture as such:

The building is a good example of late nineteenth-century industrial architecture. Covered with a shallow-pitched gable roof, the long two-story mill is characterized by rows of large, segmental-arched windows that occupy most of its four elevations. It is of solid brick construction in five-to-one common bond. A vernacular expression of the Italianate style is exhibited in the fenestration and decorative brick and millwork. Window bays are recessed to give the impression of pilasters defining the bays... [that] are further accentuated by corbelled brick brackets or wooden brackets to create a capital-like embellishment... (Oppermann 1996:2).

Today, the building continues to function as a multi-family residential complex. Though its historic windows have been replaced, the contemporary units were an attempt to match some of the original configuration and to preserve much of the historic fenestration pattern (Photo 1, p. 9). The building's massing and footprint has not changed since the mid-twentieth century when the mill was used as a warehouse for several different companies.

The interior of the building has not changed since the building's 1996 rehabilitation, containing around 50 condominiums with space converted into a parking garage at the north end. In 2005, it was noted that, "The interior retains wood floors and ceilings, and heavy wooden beams and rafters" (MAA 2005:46). The large windows continue to provide ample light and ventilation to the building's interior.



Photo 1: Southwest Oblique, Raleigh Cotton Mills (WA 3919).

At the time of its construction in 1890, the Raleigh Cotton Mill was located outside the city limits at the north end of town on a large tract of land. In keeping with other mills of the period, additional acreage was used to construct housing for many mill workers. A 1910 publication from the local Chamber of Commerce noted that a “Twenty-acre tract of meadow land adjoins the mill, giving large yards around the operatives’ cottages, and playgrounds for the children’s sports” (Chamber of Commerce and Industry 1910:29). As a plat drawn just prior to the property’s sale in 1932 illustrates, the mill company had been purchasing land in the vicinity hoping to capitalize on Raleigh’s early suburban development (Figure 4). Much of the area surrounding the mill was historically known as “Smoky Hollow” because of the environmental conditions created by the growing number of industrial properties in the vicinity, the Raleigh and Gaston Railroad engine houses, machine shops, and railroad corridor on the east, as well as the Norfolk Southern rail line located just a couple blocks west. Records from the City’s Housing authority indicate that Smoky Hollow was one of the first areas to undergo urban renewal (League of Women Voters of Raleigh 1969:5).

Presently, the Raleigh Cotton Mill is situated on 2.23 acres, bordered on the east by Capital Boulevard (previously N. Dawson Street) and on the west by what is now the CSX railroad (previously the Raleigh and Gaston, Seaboard Air Line, and Seaboard Coastline railroads). Since its conversion into a residential community, the setting of this industrial property has been softened with evergreen and ornamental landscaping. Vegetation provides a heavy screen between the building’s parking lot and Capital Boulevard.

NRHP Assessment and Boundaries

During the current survey, it was found that the Raleigh Cotton Mill retains the same architectural features and historical significance detailed in the 1999 and 2005 investigations of the property. Though a period of significance has not been previously prescribed, it is believed that the time from the mill's construction in 1890 until its sale in 1932 would encompass the significant architectural features and historic events detailed in earlier investigations where the resource was determined eligible for the NRHP. Based on the extant historic fabric from this time period, the building continues to display a high level of historic integrity in its location, workmanship, feeling and association, and a relatively moderate level in its setting, design, and materials.

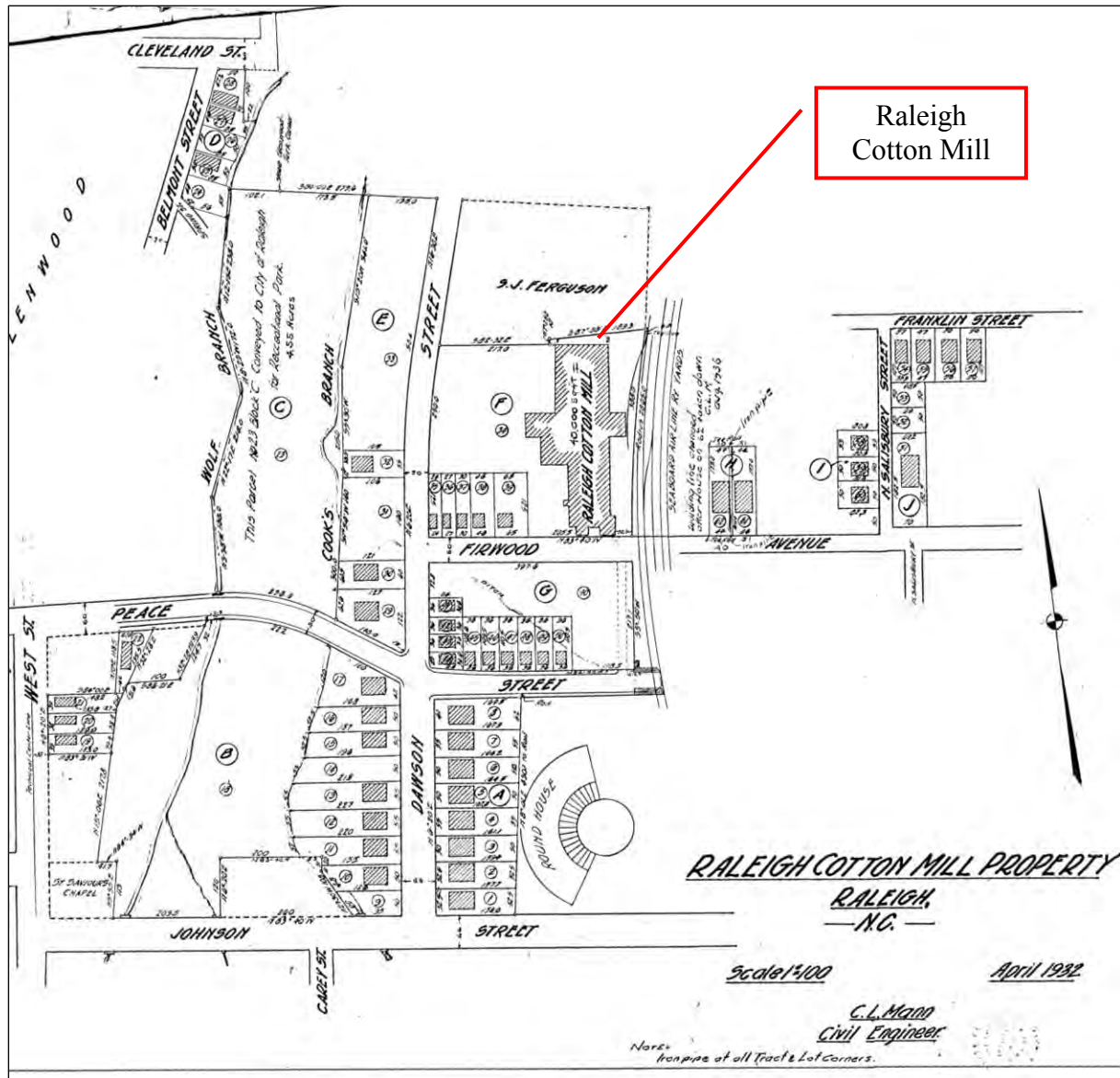


Figure 4: Plat of Raleigh Cotton Mill Property, April 1932
(C.L. Mann Map Collection on file at Wake County Register of Deeds).

Table 2: WA 3919: Raleigh Cotton Mill Assessment of Historic Integrity.

Aspect of Integrity	Level of Integrity	Assessment
Location	<i>High</i>	This resource remains in its original location.
Design	<i>Moderate</i>	Has lost some exterior features such as tower and railroad platform, but has maintained its general form, massing, and exterior architectural features. Interior features remain, though the spatial organization has been modified.
Setting	<i>Moderate</i>	Additional land and residential areas no longer surround the mill, but it remains adjacent to busy transportation corridors. Furthermore, relatively few mills from this period retain original settings.
Materials	<i>Moderate</i>	Historic windows have been replaced and some window openings have been in-filled. However, relatively few mills from this period retain original windows.
Workmanship	<i>High</i>	Original architectural details in the brick and millwork of the building remain.
Feeling	<i>High</i>	The scale and architectural features of the mill continue to express a sense of its history.
Association	<i>High</i>	The scale and architectural features of the mill continue to express a sense of its period of significance.

Given the prominence of this building in Raleigh’s industrial and manufacturing history, and the careful execution of its industrial Italianate architecture, it is recommended that this resource remains eligible for the NRHP under Criteria A and C. The NRHP boundaries established for the Raleigh Cotton Mill during previous investigations conform to the existing tax parcel, and include the 2.23 acres that surround the property (Figure 5, p. 12). These boundaries extend roughly from the railroad right of way on the east to the state-owned right-of-way along Capital Boulevard on the west, and from the property line of 622 Capital Boulevard on the north to that of the parcel at 200 W. Peace Street on the south.

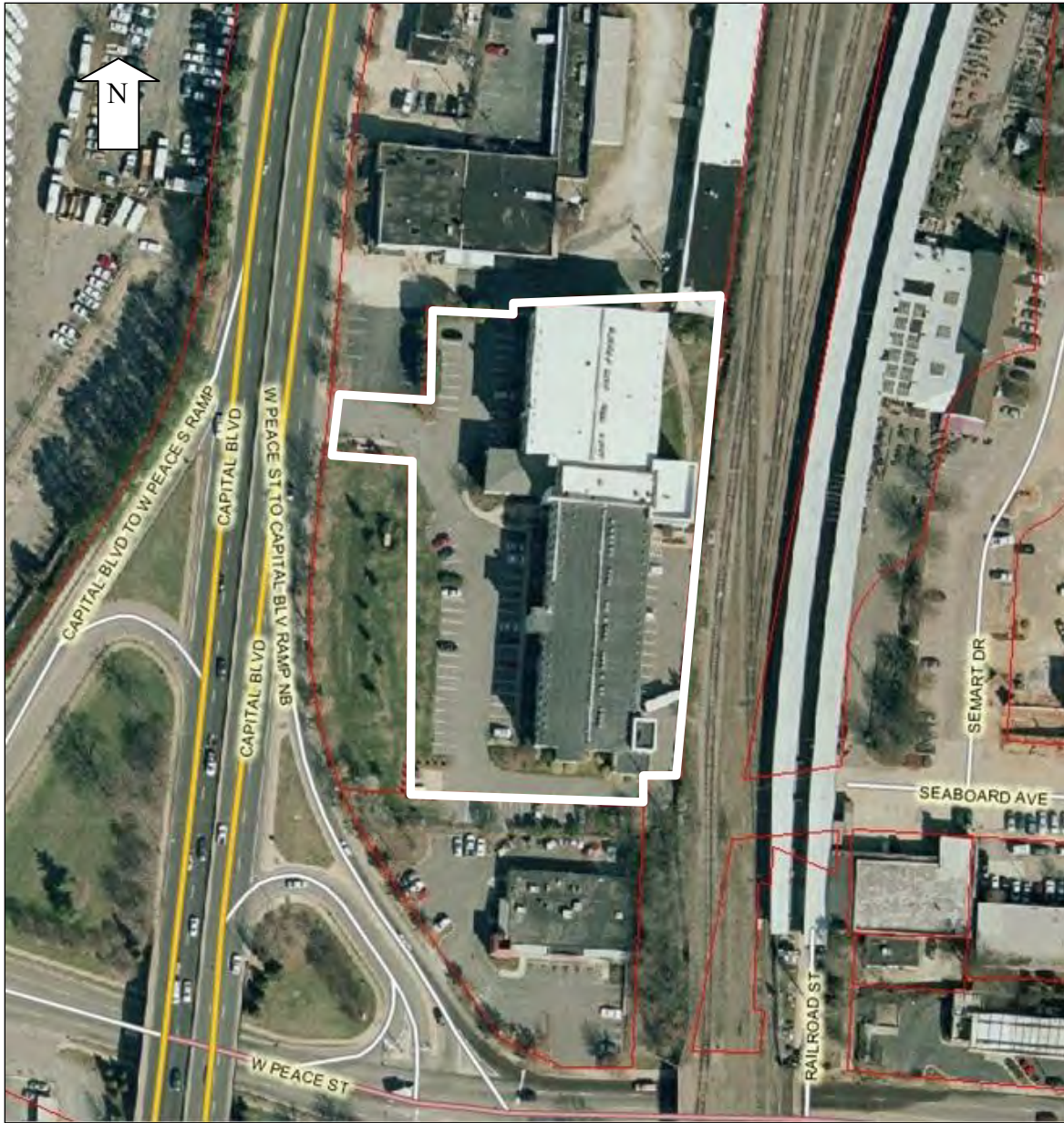


Figure 5: Map of Raleigh Cotton Mill (WA 3919) NHRP Boundaries and Current Tax Parcel, Highlighted in White (City of Raleigh and Wake County imaps 2012).

WA 7126: Noland Plumbing Company Building



1117 Capital Boulevard
Raleigh, North Carolina
PIN: 1704655538

Previous Evaluations

The Noland Plumbing Company building, addressed at 1117 Capital Boulevard, was initially surveyed by Angie Clifton in 2006, and put on the state's Study List after the publication of M. Ruth Little's "Development of Modernism in Raleigh (1945–1965)." This historic context identified the Noland Plumbing Company building as one of the best examples of Modern-era industrial distribution facilities in the city (Little 2006:36). In a letter dated September 8, 2009, the HPO concurred with recommendations of MAA, following an intensive investigation of the property, determining that the property was eligible for listing on the NRHP. In their report, "Supplement Phase II (Intensive Level) Architectural Resources Survey, Southeast High Speed Rail Corridor, Petersburg, Virginia to Raleigh, North Carolina," MAA explained that:

Under Criterion A, the building clearly represents the wave of wholesale distribution companies established in north Raleigh during the booming postwar years when such suppliers built facilities near their customers in the new subdivisions on the city's periphery. By the late-1950s, the most up-to-date commercial warehouses, such as Noland Plumbing, included stylish office and showrooms. The building is also eligible under Criterion C for architecture, ranking among the finest modernist warehouses in the city and neatly illustrating the postwar modernist movement in its linear, geometric design and sleek elevations devoid of historically derived decoration (MAA 2009b:83).

Physical Description

With the intention of prominently displaying their product, the Noland Plumbing Company hired the Raleigh architectural firm of Edwards, McKimmon, and Etheridge in 1959 to create "...a stylish modern building that combined the functions of showroom, office, and

warehouse in a design package that projected the company’s image to the general public” (Little 2006:36). Their result was thoroughly described by MAA as:

The building consists of a large, brick warehouse with a smaller office section in front, creating a modernist composition of juxtaposed geometric forms. The warehouse has a flat roof, rectangular plan, and utilitarian appearance which contrasts with the low-slung form and sleek, modernist styling of the office. The office has a flat roof with overhanging, metal eaves, supported by metal posts, that shelter the recessed façade. The front elevation is notable for both its glass and tiled walls. The south end of the office façade (east elevation) feature bays of polychromatic mosaic tiles... The glass walls north of the entrance designate the brightly lit showroom area... A customer service area, designated by a metal and glass door and ribbon windows, and truck loading bays are found along the side (south) elevation of the warehouse (MAA 2009b:82).

Today, the exterior of the Noland Plumbing Company building appears much the same as depicted in recent studies. However, the building is presently occupied by a company called East Carolina Metal Treating. Signature features of the building’s International architectural style, like the glass curtain wall along the façade, cantilevered porch, and exposed steel framing, remain unaltered since initial construction (Photo 2). Historic aerial photographs indicate that the western loading bays and rear massing is an addition to the building that was made after 1971 (Photo 3, p. 15). A black canvas awning was added to the “customer service area” along the south elevation at some point following the 2009 investigation.

During the 2009 investigation, MAA noted that, “The well-preserved interior includes offices on the southe side and a large showroom on the north side of the building. The office area has mosaic tile walls areas [sic] and original light fixtures” (MAA 2009b:82). These spaces remain in use today, however, the glass showroom appears to have been converted into additional office space.



Photo 2: View of East Elevation, Noland Plumbing Company (WA 7126)
Looking Northwest.

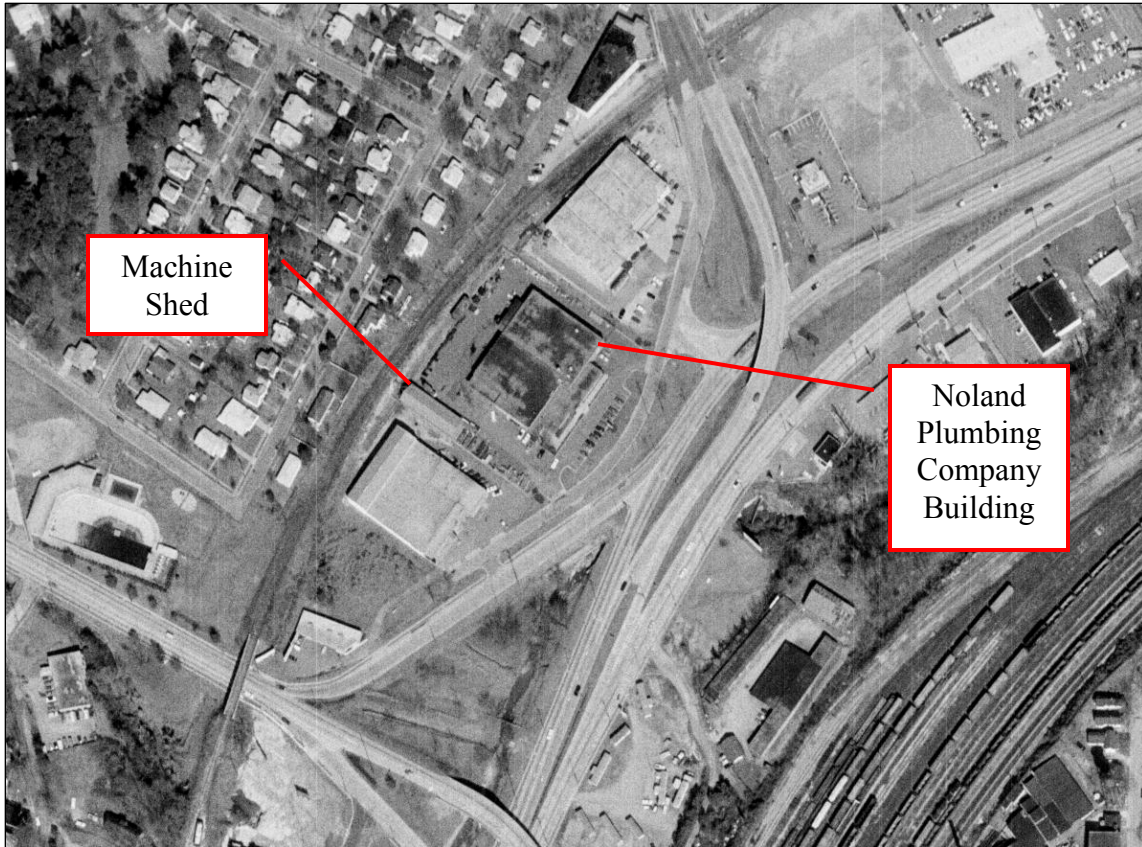


Photo 3: Aerial Image of Noland Plumbing Company Building, 1971 (United States Department of Agriculture [USDA], Historic Aerial Photos, UNC Library 2012).

Situated on a 2.5-acre parcel located on the west side of Capital Boulevard, just north of the Wade Avenue bridge, the property's setting has also remained relatively unchanged over the last 50 years. The 1971 aerial photograph depicted minimal vegetation, with only a row of shrubbery visible at the rear, or western edge of the site, along the Norfolk Southern railroad. Contrasting this historic image with today's satellite imagery reveals that the parking lot retains the same configuration and indicates that the machine shed at the southwest corner of the lot was an early addition to the site. The six-bay machine shed rests on a poured-concrete pier foundation, with a steel-framed structural system that is covered by a side-gabled roof and clad in corrugated metal on the east, south, and portions of the west elevation.

NRHP Assessment and Boundaries

During the current survey, it was found that the Noland Plumbing Company retains the same architectural features and historical significance detailed in the 2006 and 2009 investigations of the property. Though a period of significance has not been previously prescribed, it is believed that the time from the building's construction in 1960 until its sale, sometime after 2009, encompasses the significant architectural features and important historic events detailed in earlier investigations where the resource was determined eligible for the NRHP. Based on the extant historic fabric from this period, the building continues to display a high

level of all seven aspects of historic integrity: location, setting, design, materials, workmanship, feeling and association.

Table 3: WA 7126: Noland Plumbing Company Assessment of Historic Integrity.

Aspect of Integrity	Level of Integrity	Assessment
Location	<i>High</i>	This resource remains in its original location.
Design	<i>High</i>	Exterior features remain intact and largely unaltered. The rear addition does not substantially detract from the original design, and preserves the character-defining elements and elevations. Interior features remain, though the spatial organization may have been modified.
Setting	<i>High</i>	The general setting of this resource remains unaltered since initial construction.
Materials	<i>High</i>	This resource retains almost all original materials in original locations.
Workmanship	<i>High</i>	Original architectural details remain intact to exhibit workmanship throughout the building.
Feeling	<i>High</i>	The scale and architectural features of the building continue to express a sense of its history.
Association	<i>High</i>	The architectural features and site continue to invoke historical associations relevant to the period of significance.

Edwards, McKimmon, and Etheridge’s International-styled design at the Noland Plumbing Company building stands out among the light-industrial architecture of the City as an excellent example of this type of building and architectural style. Furthermore, as a mid-century warehouse and distribution facility, the Noland Company building reflects a larger historic trend that encouraged many distribution companies to relocate from downtown to the north end of the city. Registration requirements established in Raleigh’s Modernist context indicate that an industrial resource from this period should retain “...their basic integrity of design, materials, and workmanship, but should have significant architecture that exhibits a progressive design” (Little 2006:36). Few resources in the City better exhibit this progressive

intension than the Noland Plumbing Company building, and therefore, it is recommended that this resource remains eligible for the NRHP under Criteria A and C. The NRHP boundaries established for the Noland Plumbing Company building during previous investigations conform to the existing tax parcel, and include the 2.5 acres that presently surround the property (Figure 6). These boundaries extend roughly from the railroad right of way on the west to the Capital Boulevard service road on the east, and from the property line with 1125 Capital Boulevard on the north to that of the parcel at 1113 Capital Boulevard on the south.



Figure 6: Map of Noland Plumbing Company (WA 7126) NHRP Boundaries and Current Tax Parcel, Highlighted in White (City of Raleigh and Wake County imaps 2012).

Resources Recommended Eligible for the NRHP

As a result of this investigation, one historic property is recommended eligible for the NRHP as an individual resource: the Gulf gas station (WA 7363). This property was newly surveyed as a result of this undertaking.

WA 7363: Gulf Gas Station



915 Capital Boulevard
Raleigh, North Carolina
PIN: 1704641482

History

The Gulf gas station, currently addressed at 915 Capital Boulevard, is a one-story service station built in International style. It was an early development along the Downtown Boulevard, later renamed Capital Boulevard, when it was erected in 1958. Following the lot's purchase by Tremarco Corporation from members of the Turk family, the property was leased for a period of 25 years to the Gulf Oil Corporation (Wake County Deed Book [WCDB] 1273:401; WCDB 1300:28). The "Gulf Service Station" first appeared in the Raleigh city directory of 1958, and continued to function under the corporate banner until the early 1970s. Names of local operators during this period are largely unknown, but reference to "Williams Gulf Service Station" and "Hobby's Gulf Service Station and U-Haul" appear in a number of city directories. Beginning in 1972, the property transitioned into a privately operated service station, and became associated with various names through the following decades. Most recently, the building was occupied by Karl's Van Rental.

Physical Description

The one-story, three-bay, International station building rests on a poured, concrete-slab foundation, and has a masonry structural system composed primarily of concrete block. It is covered by a low-pitched shed roof that slopes gently to the west, towards the rear elevation, and which is surrounded by a parapet wall. The primary elevations are the north and east sides of the building that face on-coming traffic headed south along Capital Boulevard. These

primary elevations are clad in enameled metal panels, topped by three rounded bands, giving the building a distinct “streamlined” appearance. A flat-roofed canopy projects from the northeast corner of the building, and is supported by two slender metal poles (Photo 4).

At the south end of the east elevation are two service bays, currently filled by overhead, aluminum, garage doors. The sales area fills the northeast corner of the building, and is marked by a single-leaf wood door and large, metal-framed display windows. Two restrooms are accessed from the exterior of the building, both located near the northwest corner of the station. The south and west elevations of the building are unadorned and dotted by multi-light, steel-framed windows.

The interior of the building is divided into three functional spaces, the largest of which is the automobile service area (Figure 7, p. 20). This service space contains two open bays with work benches along the west wall and a ventilated storage closet at the rear. This area was largely intended for use by mechanics and is entirely utilitarian. Walls are unadorned, concrete block, and the poured-concrete floors are sloped towards drains in the center of each bay.



Photo 4: Northeast Oblique, Gulf Gas Station (WA 7363).

The most visible space within the station is the sales area, where large display windows showcase the products and personnel inside. Interior walls within the sales room are clad in stucco with tiled skirting. Original display windows come together as “glass meets glass”—a typical architectural feature of the International style (Photo 5, p. 20). The indoor/outdoor carpeting in the sales room covers asbestos tiles, while the counter appears to be of a recent vintage. The main entry door and transom window are replacements. Only one of the restrooms is in working order, but both facilities retain mid-century fixtures and ornamentation. Bathroom walls are tiled from the floor to the dropped ceiling.

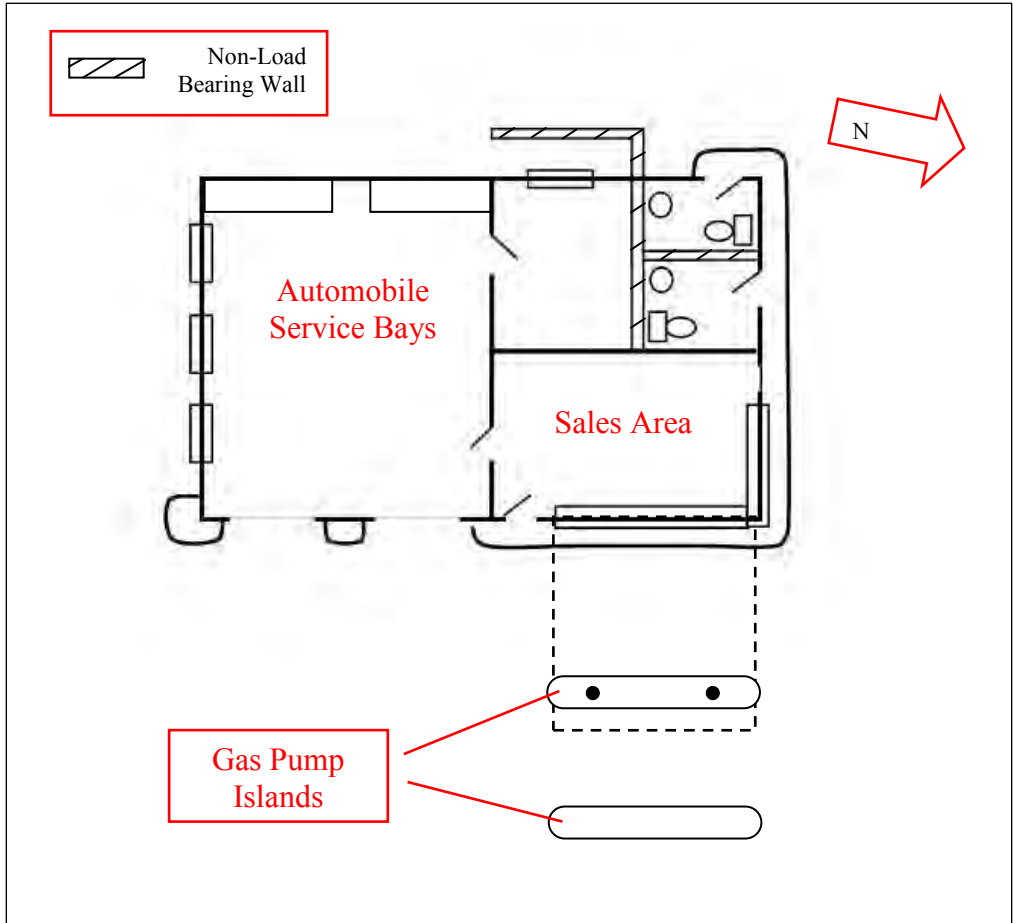


Figure 7: General Plan of Gulf Gas Station, 915 Capital Boulevard (WA 7363).



Photo 5: At Left, Interior View of Sales Area, Looking East Towards Capital Boulevard; at Right, Interior View of Bathroom, Gulf Gas Station (WA 7363).

Situated on a 0.65-acre parcel located on the west side of Capital Boulevard, just south of the Wade Avenue bridge and north of Dortch Street, the property’s setting has not changed significantly since the building was constructed in 1958. The majority of the lot remains paved, with vegetation regulated to western edge of the lot along Pigeon House Branch. Contrasting historic aerial images, a late-twentieth-century, prefabricated trailer, situated just north of the station building, appeared to be vacant at the time of survey.

NRHP Assessment and Boundaries

Gas stations were noted as an “important suburban building type” in Little’s 2006 Modernist context for the City of Raleigh, although just one example was surveyed in association with the study. This resource, an Esso station at 2121 Garner Road, was prized for its “flat-roofed box with white enameled steel sheathing...” among other Modern architectural features (Little 2006:33). A reconnaissance survey of this building revealed that it has since been remodeled and its metal paneling removed (Photo 6).



Photo 6: Northeast Oblique of Gas Station at 2121 Garner Road (October 2011).

Additional research on mid-century gas station design uncovered W. Dwayne Jones’ 2003 publication for the Texas Department of Transportation (TXDOT) entitled, “A Field Guide to Gas Stations.” This contextual study reported on the gas station’s historical development during the twentieth century, detailed the architecture of several important oil companies, and established registration requirements for these resources within the state of Texas. The report identifies the station at 915 Capital Boulevard as a standard building type of the Gulf Oil Corporation, popular during a period of great expansion for the company from 1950 until the early 1970s (TXDOT 2003:87) (Figure 8, p. 22). Significant architectural features of this station type built upon an earlier corporate design, dating to the late-1940s, that exhibited International or Modern styling, an oblong box form, a flat roof “with three distinctive blue

bands encircling the building,” and white-enameled porcelain sheathing” (TXDOT 2003:68) (Photo 7). By the 1950s, Gulf stations frequently featured two service bays, a flat-roofed canopy resting on two slender metal poles, large corner windows at the sales space, and a “second detached island of gasoline pumps...” (TXDOT 2003:87).

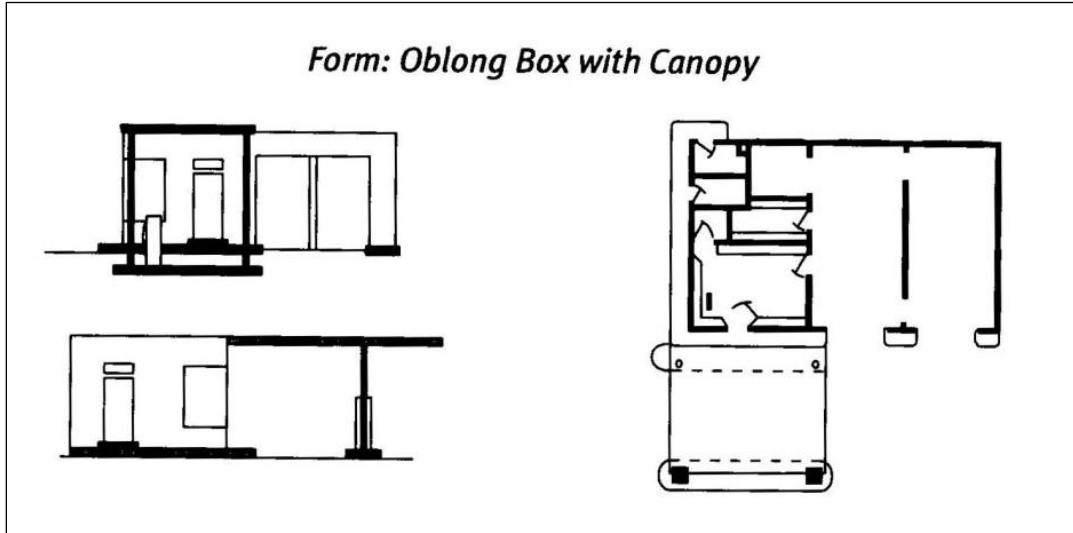


Figure 8: Architectural Drawing of Oblong Box Form Commonly Employed in Gulf Company Standard Station Design, 1950 –1970 (TXDOT 2003:87).



Photo 7: Southeast Oblique, Gulf Gas Station (WA 7363), With View of Distinctive “Gulf” Bands. Note the touch of the company’s trademark blue visible under the red paint.

Based on the extant historic fabric from this period, the building continues to display a relatively high level of all seven aspects of historic integrity: location, setting, design, materials, workmanship, feeling and association. Although most all of the doors appear to have been replaced and the gas pumps have been removed, these alterations do not substantially diminish the building’s integrity of materials, workmanship, association or feeling. The station on Capital Boulevard’s design and materials are in keeping with the Gulf Company’s standard model at the time it was constructed in 1958, therefore, the suggested period of significance highlights the time when it functioned as a corporate service station, from 1958 to 1972.

Table 4: WA 7363: Gulf Gas Station Assessment of Historic Integrity.

Aspect of Integrity	Level of Integrity	Assessment
Location	<i>High</i>	This resource remains in its original location.
Design	<i>High</i>	Both exterior and interior features remain intact and largely unaltered.
Setting	<i>High</i>	The general setting of this resource remains unaltered since initial construction.
Materials	<i>High</i>	This resource retains most all original materials in the original locations. Only doors, and one transom window appear to be replacements.
Workmanship	<i>High</i>	Original architectural details remain intact to exhibit workmanship throughout the building.
Feeling	<i>High</i>	The scale and architectural features of the building continue to express a sense of its history.
Association	<i>High</i>	The architectural features and site continue to invoke historical associations relevant to the period of significance.

Registration requirements established in Raleigh’s Modernist context indicate that a commercial building from this period should retain “...A high level of historic integrity of design, workmanship, and materials... [with] features that express the Modernist aesthetic” (Little 2006:34). The Gulf service station is an outstanding, and perhaps unique, example of a Modern-era gas station designed in the International style within the City of Raleigh. As such, it is recommended eligible for individual listing on the NRHP under Criterion C. The building has no known association with an important event or individual, so it is recommended Not Eligible for the NRHP under Criteria A or B. In addition, due to the large-scale modifications of the surrounding landscape during land development at the time of

business establishment, subsurface deposits throughout this area were greatly disturbed through the installation of subsurface gas tanks and other utilities and installation of signage. Moreover, the commonality of mid-century concrete block construction precludes the potential for below-ground remains associated with this resource to provide notable data on commercial structural technology in this area. As such, it is recommended that the resource is Not Eligible under Criterion D. Suggested NRHP boundaries conform to the existing tax parcel, and include the 0.65 acres that presently surround the building (Figure 9). These boundaries extend roughly from the right of way along N. West Street on the west to that of Capital Boulevard on the east, and from the property line with 919 Capital Boulevard on the north to that of the parcel at 909 Capital Boulevard on the south.



Figure 9: Map of Gulf Gas Station (WA 7363) NHRP Boundaries and Current Tax Parcel, Highlighted in White (City of Raleigh and Wake County imaps 2012).

Resources Recommended Not Eligible for the NRHP

As a result of this intensive-level investigation, two historic properties are recommended not eligible for the NRHP as individual resources: Finch's Restaurant (WA 4607) and the Raleigh & Gaston/Seaboard Air Line Railroad Turntable and Roundhouse Site (WA 7383).

WA 4607: Finch's Restaurant



401 West Peace Street
Raleigh, North Carolina
PIN: 1704517934

History

The historic building at 401 W. Peace Street was initially surveyed by Sarah Davis Lachenman in 2006, during an investigation into the Modernist architecture of Raleigh. Although the resource was mentioned in the resulting historic context, it was not nominated to the Study List and has not been previously evaluated for the NRHP.

Constructed in 1948 by Octavius W. Hooker, owner and operator of Economy Cleaners, this commercial building was first occupied by Roy G. Pinyoun as “Pinyoun’s Drive Inn” (Raleigh City Directory 1948). Mr. Pinyoun was the first restaurateur in town to market this type of automobile-oriented dining experience. The following year, in 1949, the restaurant was taken over by Howard J. Finch and renamed “Finch’s Drive In” (Raleigh City Directory 1949). Raleigh city directories indicate that prior to operating this restaurant, Howard Finch had been in and out of work at various jobs, spending time as a clerk (1937) and a manager at the Sir Walter Tavern (1942), working at the Center Smoker Shop (1943–46) and at Raleigh First Self-Serve Laundry (1948). Howard and his wife, Lillie Mae, had lived in a number of places since moving to town in 1934–35, but appear to have settled at 301 Perry Street by around 1945.

It is perhaps their location on Perry Street, within the same block as O. W. and Blanche Hooker, or maybe Howard Finch’s employment with the Raleigh First Self-Service Laundry, that led him to rent the newly constructed building at 401 W. Peace Street. First mention of

the restaurant is seen in the city directory of 1949 when Finch's Drive Inn advertised: "Frozen Custard, Hamburgers, Cheesburgers, Hot Dogs & Sandwiches, Curb Service From 11 AM til Midnight (Formerly Pinyoun's Drive Inn)" (Raleigh City Directory 1949). In 1950, Finch's had added "chicken in a box" to their menu, and were competing with five other restaurants in town that had begun marketing themselves as "drive ins"—one of which, named "Roy's Drive In," belonged to Roy Pinyoun. By 1952, the number of drive-ins had grown to 11 (Raleigh City Directory, 1950; 1952).

The restaurant's location on the north end of town, adjacent to the circa-1954 Downtown (Capital) Boulevard and across the street from the circa-1937 Devereux Baseball Park, may have contributed to the lasting success of this venue. Finch's role in the community was likely another significant factor. A half-page advertisement for Finch's restaurant appeared in the city directory of 1960, touting the business' relationship with several local organizations (Photo 8). The current menu at Finch's features one former employee's memories of Mr. Finch and his support of a local baseball team (Photo 9, p. 27).

In the 1960s, Finch expanded to operate at least four other restaurants in the City. Howard J. Finch remained the proprietor of Finch's restaurant until 1973, when he sold the business to Leroy Teel and George Whitman (Marshall 1988). Although the restaurant and property has changed hands a few times in the decades since, "Finch's Family Restaurant" remains in business today at its original location, serving breakfast and lunch to many regular and long-time patrons.



Photo 8: Finch's Advertisement, Circa 1960
(Raleigh City Directory 1960, Olivia Raney Local History Library).

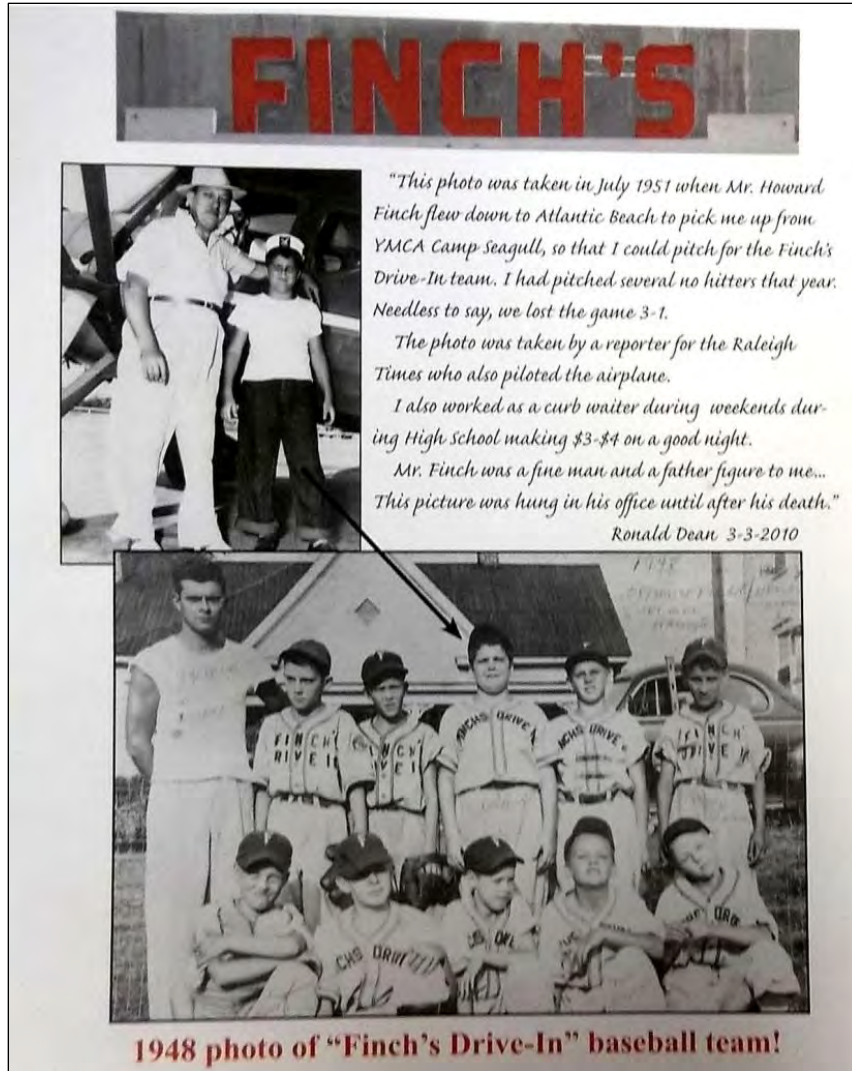


Photo 9: Historical Excerpt from Finch's Family Restaurant Menu (2012).

Physical Description

The one-story, five-bay, mid-century, Modern commercial building located at 401 W. Peace Street was constructed in 1948. It rests on a poured, concrete-slab foundation and has a cinder-block structural system. The flat roof is surrounded by a parapet wall with terra-cotta tile coping. The primary façade, or north elevation, is clad in a mixture of stretcher-bonded brick and black Carrera glass (Photo 10, p. 28). The storefront is sheltered by a continuous, flat, metal awning that stretches across the façade. The name "Finch's" is spelled in metal letters that are painted red, and appear to have originally contained neon lighting (Photo 11, p. 28).

An addition was made to the west side of Finch's restaurant in the early-1950s that connected it with the warehouse building next door at 403 W. Peace Street. At one point in time, a portion of this neighboring structure was reportedly used as a "cafeteria" in association with Finch's restaurant, but this expansion appears to have been short-lived.



Photo 10: North Elevation of Finch's Restaurant (WA 4607).



Photo 11: Detail of Façade and Sign, North Elevation of Finch's Restaurant.

Finch's building and its surrounding site have endured a number of physical modifications since construction in 1948. Sanborn Fire Insurance Company maps from 1950 show the building in its earliest period, as a one-story, cinder-block restaurant with a brick façade, asphalt composition roof, 12-inch parapet wall (Figure 10, p. 29). The façade and part of the west elevation was then sheltered by a wrap-around metal awning, without the corrugated metal canopy that currently dominates the building's façade. At the time of its construction, Finch's was surrounded by a relatively low- to moderate-income neighborhood that grew up

on the edge of town in an industrial area. Known to locals as “Smoky Bottom,” this area was the first in town to be remediated by urban renewal efforts in the mid-1950s (League of Women Voters Raleigh-Wake County 1969).

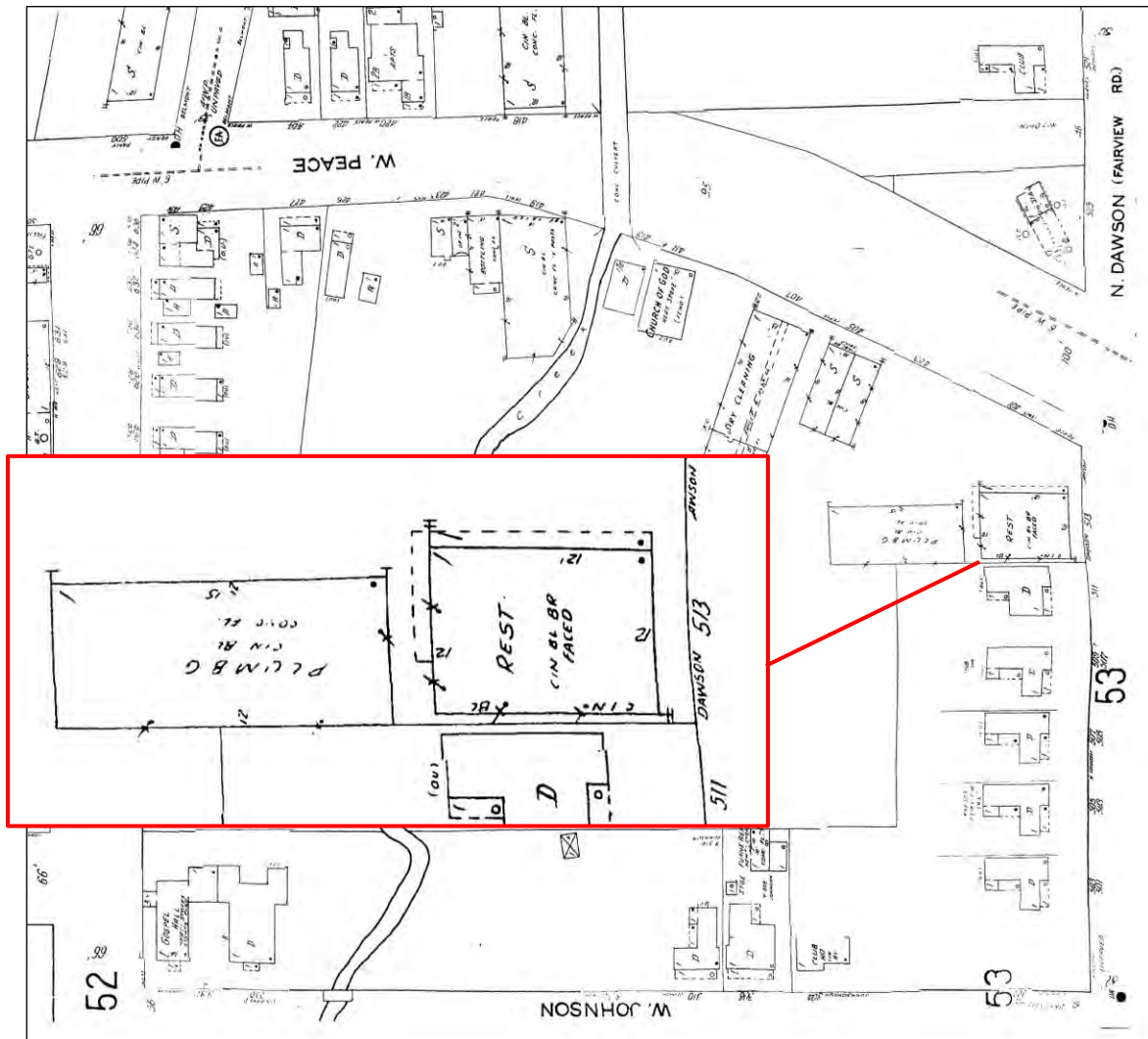


Figure 10: Sanborn Fire Insurance Map, Revised to 1950 (ProQuest 2001). According to the Raleigh City Directory of 1949 and 1950, detail in red contains Finch’s Drive Inn (“Rest” on right) and South Heating and Electric Company (“Plumbg” on left).

Aerial photographs taken during the construction of Downtown Boulevard in the early-1950s indicate that the one-bay, west-side addition connecting Finch’s to the Heating and Electric Company building was an early alteration to the resource (Photo 12, p. 30). Based on communication with the current owner, this west-side addition was reportedly constructed to better facilitate the fast pace of curb-side service, provided by a double-leaf entrance and sizable “order up” window. Other modifications to the building include a concrete-block, lean-to addition on the south elevation, with another frame addition extending this later addition further west along the rear of the property (Photo 13, p. 30).



Photo 12: Detail of Finch's, *News and Observer* Aerial (on file, North Carolina State Archives). Certain elements suggest a date of 1953 or 1954.



Photo 13: View of Rear Lean-to Addition, Looking West from Capital Boulevard Sidewalk.

Divided into private “service” and public “served” spaces, the interior of Finch’s restaurant retains historical associations with mid-century diners. The building is largely split into dining space and private food preparation and storage space, with the public areas receiving the most ornament and updates over time (Figure 11). The most noticeable historic feature in the dining room is the counter and bar stools, along with the grill and stainless-steel food preparation area behind it (Photo 14, p. 32). Patrons’ bathrooms retain original tile and some fixtures from the period. However, significant alterations, such as the addition of particle-board paneling, carpeting, and loss of original booths, diminish the historic integrity of design, material, workmanship, feeling, and association within the main dining area (Photo 15, p. 32).

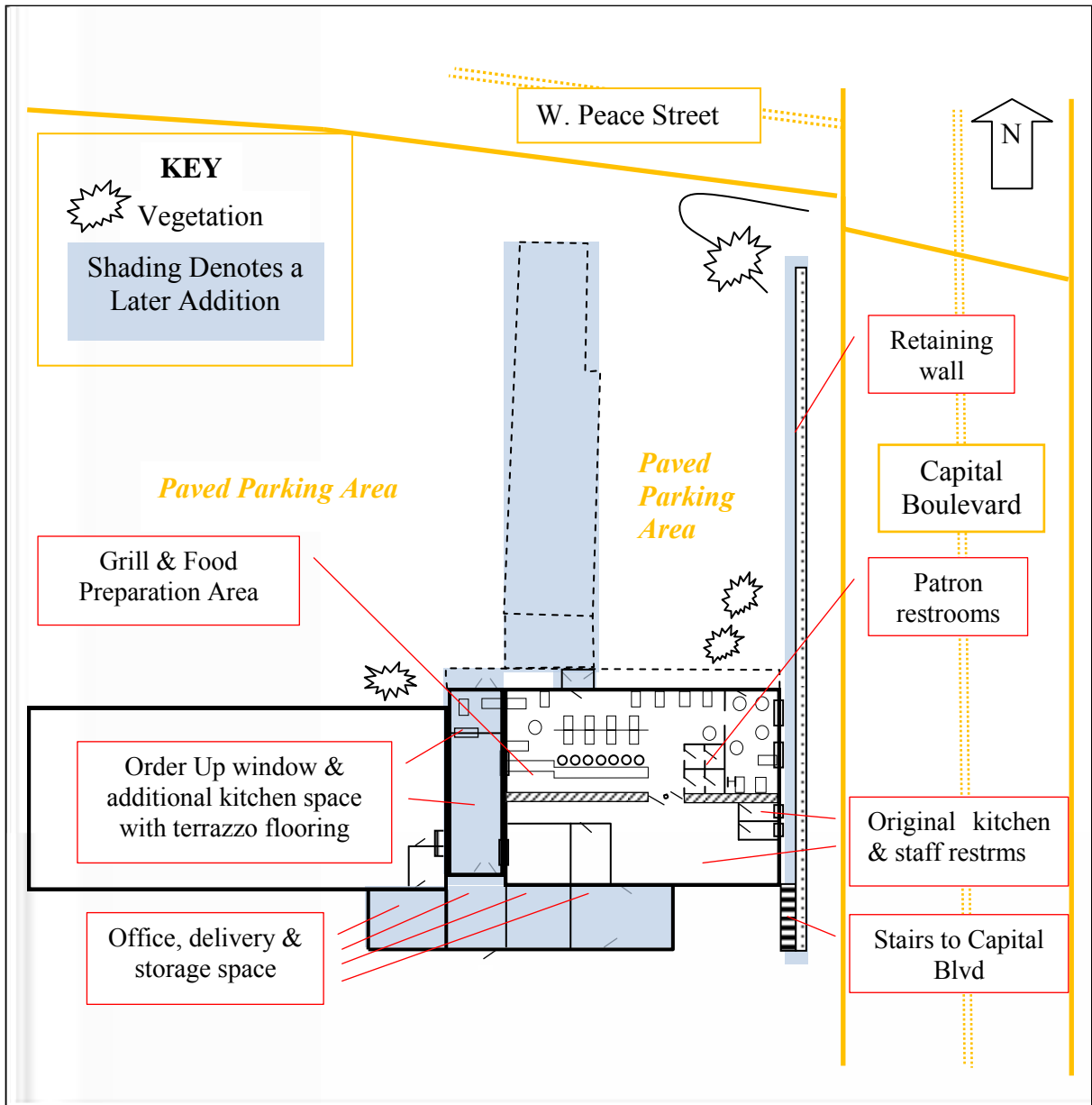


Figure 11: General Site Plan and Interior Arrangement at Finch’s Restaurant.



Photo 14: View of Counter and Bar Stools, Finch's Restaurant.



Photo 15: At Left, View of Main Dining Area and At Right, View of Patron Restroom.

Modifications in the private service spaces of the building include the kitchen floor and some walls that have been covered by terra-cotta-colored tile. Some kitchen equipment appears to be of recent vintage, and the staff restrooms are in fair to poor physical condition (Photo 16, p. 33). Taken on the whole, work areas within the original footprint of the building remain relatively intact. The lean-to addition to the rear of the building has been modified over time to suit contemporary refrigeration and food storage regulations.



Photo 16: At Left, View of Staff Restroom, and at Right, View of Hallway Leading from Kitchen to Storage Spaces and Terminating at West-Side Kitchen Addition.

The additional kitchen space added on to the west side of the building in the early 1950s appears to have been intended for improving curb-side service. A single-leaf doorway allowed the person at the grill behind the counter in the main dining area to cross over into the added kitchen area, while a large “order up” window (now covered by curtains in the small, west-side dining room) and double-leaf doors permitted a quick form of egress, giving wait-staff easy access to both the food and patrons (Photo 17 and Photo 18, p. 34).



Photo 17: Interior View of Added Kitchen Space, Looking North.



Photo 18: At Left, Exterior View of Added West Bay, and at Right, View of Grill Cook's Doorway and Windows from West Elevation of the Original Building. Note painted "Curb Service" and "Cafeteria" signage on façade.

Perhaps more so than the building, the setting surrounding Finch's restaurant has changed dramatically since 1948. Construction of Capital (Downtown) Boulevard required the demolition of numerous dwellings along what used to be N. Dawson Street and altered the layout of neighboring streets. The realignment of the intersection between W. Peace and N. Dawson Street cost Finch's the enhanced visibility provided by the corner lot, and substantially restricted the passage of automobiles through the parcel. The new roadway also necessitated the construction of a large retaining wall along the east side of Finch's lot, as well as the installation of metal stairs at the southeast corner of the property to provide pedestrian access from the new elevated sidewalk (Photo 19).



Photo 19: Views of Retaining Wall and Stairway to Capital Boulevard, Looking South.

Another important modification to Finch’s setting over time affects the “drive-in” nature and arrangement of the parking lot. Early photographs indicate that on-site parking was organized much like a shopping center, with cars lined up in rows running parallel with the building’s façade. Aerial images reveal that the metal canopy and parking arrangement changed sometime between 1959 and 1971 (Photo 20). This later configuration remains in place today, although curb-side service has not been offered in more than twenty years.

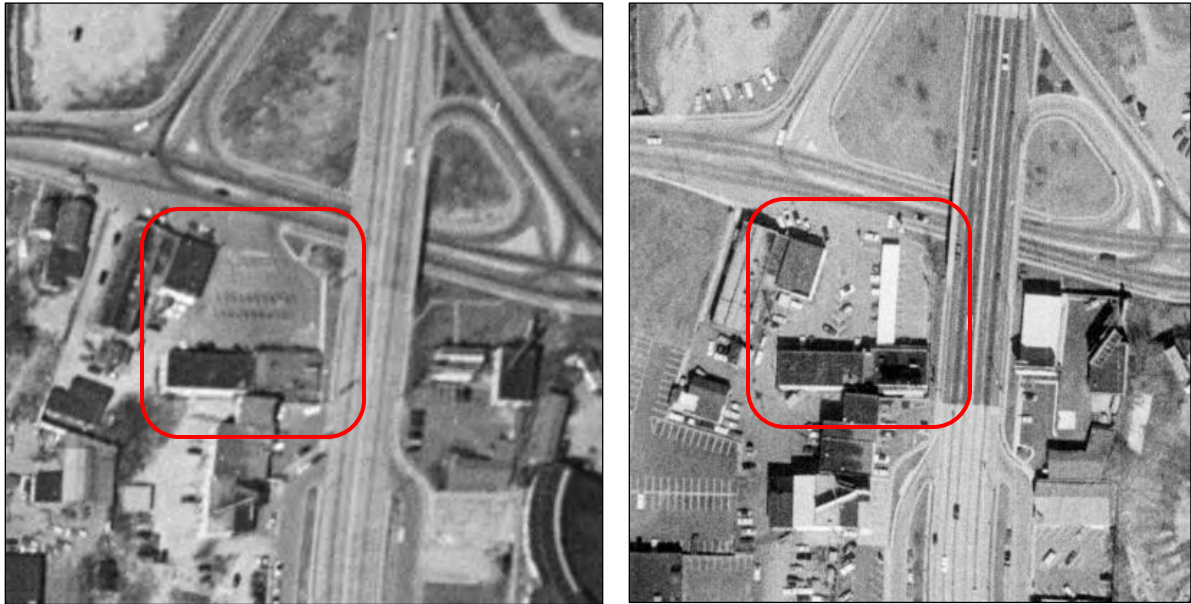


Photo 20: 1959 (Left) and 1971 (Right) Aerial Photograph of Finch’s Restaurant and Immediate Vicinity (USDA Historic Aerial Photos, UNC Library).

NRHP Assessment

The commercial building at 401 W. Peace Street has been filled by Finch’s Restaurant since 1949, and was one of three historic “drive-in” restaurants mentioned in “The Development of Modern Architecture in Raleigh, 1945–1965.” During their investigation, author M. Ruth Little pointed out that “...Finch’s Restaurant with its long pipe-columned canopy in front is a combination eat-in restaurant and drive-in” (Little 2006:33). While this assessment remains accurate, Finch’s building type started out as more a diner than a drive-in, with subsequent modifications designed to modernize the facility continuing throughout the 1950s and 60s.

Raleigh’s Modernist context stipulates that commercial buildings from this period must maintain “A high level of integrity in design, workmanship, and materials...for National Register eligibility of individual buildings.” Further, “It is important that the features that express the Modernist aesthetic still be in place” (Little 2006:34). Extant features at Finch’s which reflect the Modernist movement include the Carrera glass storefront and flat metal awning that give it a fashionable streamlined look. The brick planters do not appear to be an original feature of the façade. Another later addition is the projecting metal canopy that covers a concrete walkway. It is topped with a deeply grooved, corrugated metal sheathing and supported by “angled pipe columns”—features that somewhat detract from the styling of

the streamlined storefront. From an architectural perspective, the building continues to convey a muddled and subdued expression of the Modernist aesthetic on the exterior. However, modifications to the interior have all but obscured the streamlined diner aesthetic.

Taken on the whole, Finch's historic integrity of design, materials, workmanship, and setting have been diminished by the accretion of additions to the façade, historically insensitive alterations to the interior, damaged architectural fabric, and site modifications over time. This loss further impairs the building's historic integrity of feeling and association. Registration requirements within the City's Modernist context indicate that interior spaces are not as important to a building's significance as the exterior features, noting that "...the building's iconic power, its street presence, *may* outweigh interior alterations" (Little 2006:34). Today, the physical features of Finch's Restaurant do not convey the bold design and street presence emphasized in the Modernist context. Bold design elements, like over-sized signage, abundant use of lighting, and eye-catching architectural features are more clearly exhibited by other Modern-era restaurants in the City of Raleigh, and the State of North Carolina at large. In town, extant drive-ins, like Char-Grill and Chip's, demand drivers' attention through their architecture and sizable signage (Photo 21). Therefore, it is recommended that this resource be considered not eligible for individual listing on the National Register of Historic Places under Criterion C.



Photo 21: Extant Modern-era Restaurants in Raleigh (Top) and Charlotte (Bottom), North Carolina (Roadside Architecture.com 2012).

National Register Bulletin 15, “How to Apply the National Register Criteria for Evaluation,” presents the following advice for applying those Criteria informed by a property’s associative values (A and B):

Criterion A recognizes properties associated with single events, such as the founding of a town, or with a pattern of events, repeated activities, or historic trends, such as the gradual rise of a port city's prominence in trade and commerce. The event or trends, however, must clearly be important within the associated context... Moreover, the property must have an important association with the event or historic trends, and it must retain historic integrity (National Park Service [NPS] 1990:12).

Bulletin 15 goes on further to state that:

Mere association with historic events or trends is not enough, in and of itself, to qualify under Criterion A: the property's specific association must be considered important as well. For example, a building historically in commercial use must be shown to have been significant in commercial history (NPS 1990:12).

After a review of local newspapers, historical publications, and conversations with Finch’s patrons, it was determined that the restaurant is a long-standing landmark in the City of Raleigh, but it is not known to have been a significant factor in its commercial development in the mid-twentieth century. Though it is notable as the first restaurant to market itself as a “drive-in” the credit for its creation actually belongs to Roy Pinyoun, and the loss of historic integrity of setting and design significantly diminishes this association. Other buildings within the City exhibit stronger associations with the drive-in restaurant as a trend in dining and commercial industry, with Char-Grill at 618 Hillsborough Street being the finest example and one which continues to function as a restaurant. Therefore, Finch’s Restaurant is also recommended not eligible for the NRHP under Criterion A.

Howard J. Finch’s obituary, published in the News & Observer on June 23, 1974, indicates that Mr. Finch had once been elected “Restaurateur of the Year” and served for a time as President of the North Carolina Restaurant Association. Although Finch appears to have been well-known to his patrons, the local baseball community, and active in the state’s restaurant association, his importance could not be demonstrated to have significantly influenced local, state, or national history. The property is not known to be associated with any other important individuals in the past, and therefore, this resource is also recommended not eligible under Criterion B.

In addition, the aforementioned modifications to the building and surrounding parcel, as well as the installation of utilities, have greatly altered the subsurface integrity of the lot as well as the soils directly surrounding the above-ground resource. These changes have resulted in large-scale subsurface disturbances, thus obliterating the original builder’s trench and almost all opportunity to learn from the below-ground evidence of mid-century diner structural technology. As such, it is recommended that the resource is Not Eligible under Criterion D.

Table 5: WA 4607: Finch’s Restaurant Assessment of Historic Integrity.

Aspect of Integrity	Level of Integrity	Assessment
Location	<i>High</i>	This resource remains in its original location.
Design	<i>Moderate to Low</i>	The form, scale, and massing of the building has been minimally altered over time, but the façade still reads as a Modern-era resource. At the interior, historic features remain to convey the “diner” experience, but are diminished by subsequent additions and alterations.
Setting	<i>Low</i>	The setting of the resource has changed substantially since initial construction.
Materials	<i>Moderate to Low</i>	This resource retains many exterior materials from the period of significance, but these features are in fair to poor condition. Materials at the interior have been significantly compromised by alterations over time.
Workmanship	<i>Low</i>	Few architectural features remain to exhibit workmanship in the building, and some of them have been damaged over time.
Feeling	<i>Moderate to Low</i>	The scale and architectural features of the building continue to express a sense of its history, but have been muddled by recent alterations.
Association	<i>Moderate to Low</i>	The physical fabric of the property continues to invoke some historical associations relevant to the period of significance.

WA 7383: Seaboard Air Line Turntable and Raleigh & Gaston Railroad Roundhouse Site



Johnson Street Rail Yards (31WA1448)
W. Johnson Street
Raleigh, North Carolina
PIN: n/a (X: 2106244, Y: 741700)

History

Located at the northwest corner of W. Johnson Street and the CSX railroad tracks, a circa-1917 turntable is still in service, supporting the functions of this railroad corridor. The CSX railroad corridor provides much of the east side boundary for the project APE, and has belonged to a number of companies in the last century. Presently owned by the Consolidation Seaboard Expanded (CSX) Transportation Corporation, the corridor began as part of North Carolina's earliest railroad, the Raleigh and Gaston (RGR). This rail line has played a significant role in the development of the capital City since the RGR began operations in 1840 and has continued to influence the city under ownership of the Seaboard Air Line (SAL), Seaboard Coastline, and the CSX transportation corporation into the twenty-first century (MAA 2007:10, 22).

In 2007, a Phase II architectural investigation determined that the RGR corridor was Eligible for the NRHP, including the "...intact railroad alignment and grade and the remaining rail-related features between Raleigh and Norlina all contribute to the significance and integrity of the railroad corridor" (MAA 2007:39). The same report specifically discussed the SALR turntable, noting an interview during which the resource was determined Eligible by staff of the Office of State Archaeology (OSA) (MAA 2007:29). The NRHP boundaries for the RGR corridor recommended in this report include the turntable along with the roundhouse site and other historic sites in the Johnson Street rail yards (Figure 12, p. 40). Communication with the HPO indicates that the railroad corridor was not given a specific architectural survey number. However, the Johnson Street rail yard complex has been recorded with the OSA as site number 31WA1448. This site includes the turntable as well as the area south and east of the structure that was historically associated with the Johnson Street rail yard (Webb and Mills 1999:74).

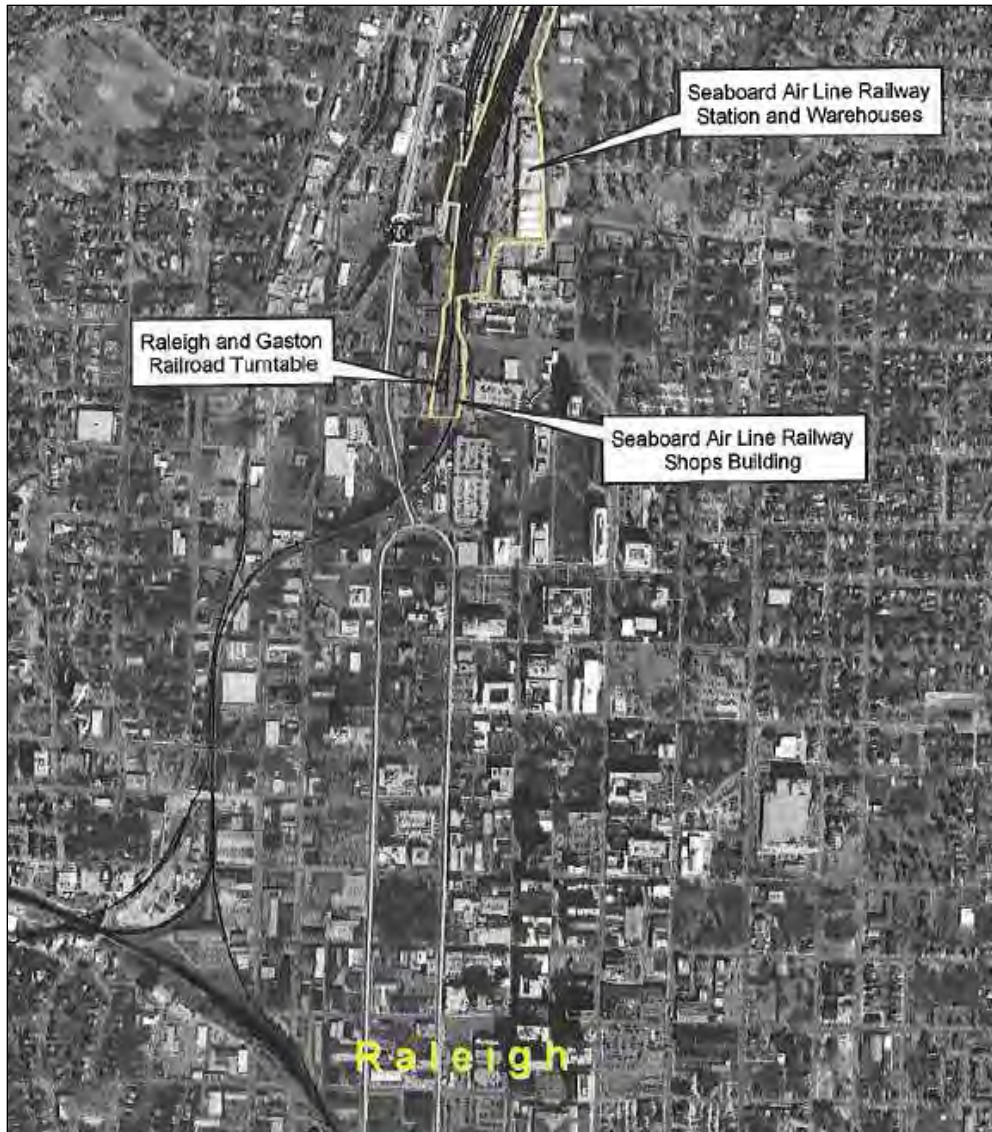


Figure 12: “Raleigh and Gaston Railroad/Seaboard Air Line Railway Proposed National Register Boundaries,” June 5, 2007 (MAA 2007:40).

The extant turntable and surrounding roundhouse site was part of this major hub of railroad activity in the late-nineteenth and early-twentieth century that has nearly vanished from the city center. Numerous architectural and archaeological studies have recently examined the history of this railroad corridor, but few have thoroughly inspected the historic buildings and structures at the Johnson Street rail yards in Raleigh. Perhaps the most thorough investigation of this rail yard comes from a March 1990 report by the Cultural Resource Group (CRG) entitled “Excavation of the Raleigh & Gaston Railroad Roundhouse,” on file at the Office of State Archaeology. This report describes the growth of the RGRR throughout the nineteenth century, and its continuing operations in Raleigh after it was consolidated into the Seaboard Air Line Company (SAL) between 1893 and 1900. Though it specifically details the first RGRR roundhouse, located southeast of the extant turntable and truly round in shape, this report also contains contextual information on the development, inner-workings, and built

environment of American railroad yards during the later-half of the nineteenth and into the mid-twentieth century.

The first RGRR roundhouse and machine shops were constructed in the early 1860s, but not finished until 1870 (CRG 1990:2). Shortly after this complex was completed, the company outgrew it. The circa-1870 roundhouse became too small for the latest engines, so the RGRR company began construction on a second engine house in 1886 (CRG 1990:15). This second model was also referred to as a “roundhouse,” but was actually half of a roundhouse, functioning more like an open shed than the original, enclosed building. The new building, located on the west side of the racks just north of Johnson Street, contained 18 engine stalls, and was “...constructed with an iron and wood front, brick rear wall, iron rafters, [and] slate roof...” (CRG 1990:15). Both roundhouses are detailed in Shaffer’s 1888 map of Raleigh (Figure 13). Each building also appears on the Sanborn maps of 1888 and 1914, but by 1950, only the half-roundhouse remained standing.

The colored Sanborn map of 1914 illustrates the architecture of the second, circa-1886 roundhouse at its largest and includes a turntable that measures approximately 80 feet in length (Figure 14, p. 42). An archaeological investigation into the original roundhouse reports that both structures were routinely damaged by fire. However, the half-roundhouse appears to have survived the longest, and is visible in aerial photographs as late as 1959 (Photo 22, p. 42).



Figure 13: Detail of RGRR Roundhouses from “Shaffer’s Outline Map of the City of Raleigh, N. C.” (Shaffer 1888).

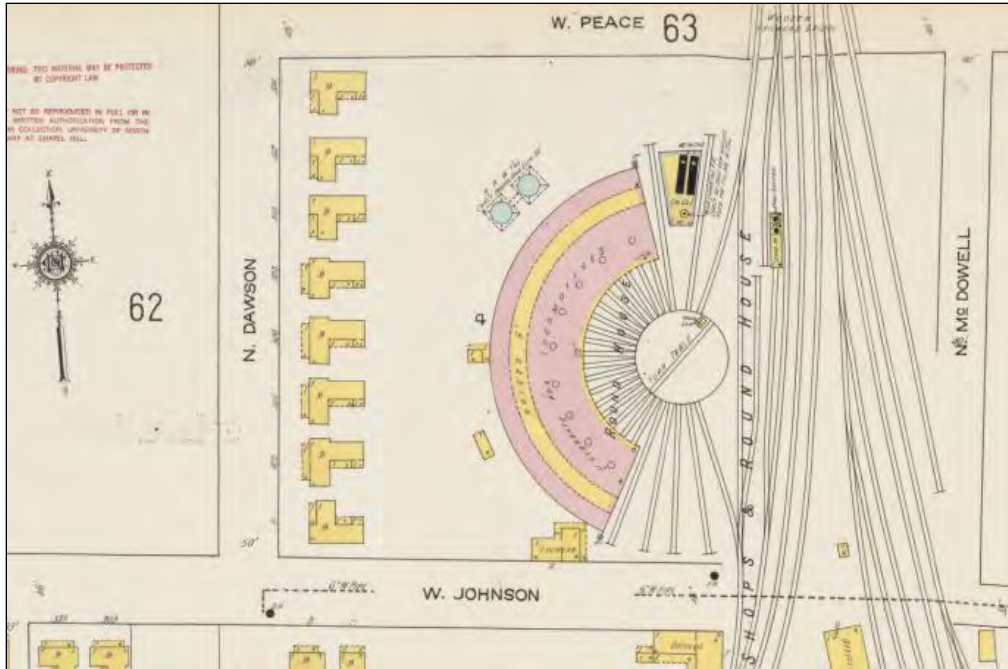


Figure 14: 1914 Sanborn Fire Insurance Map of SAL Turntable and [Second] Roundhouse (North Carolina Sanborn Maps 1914).

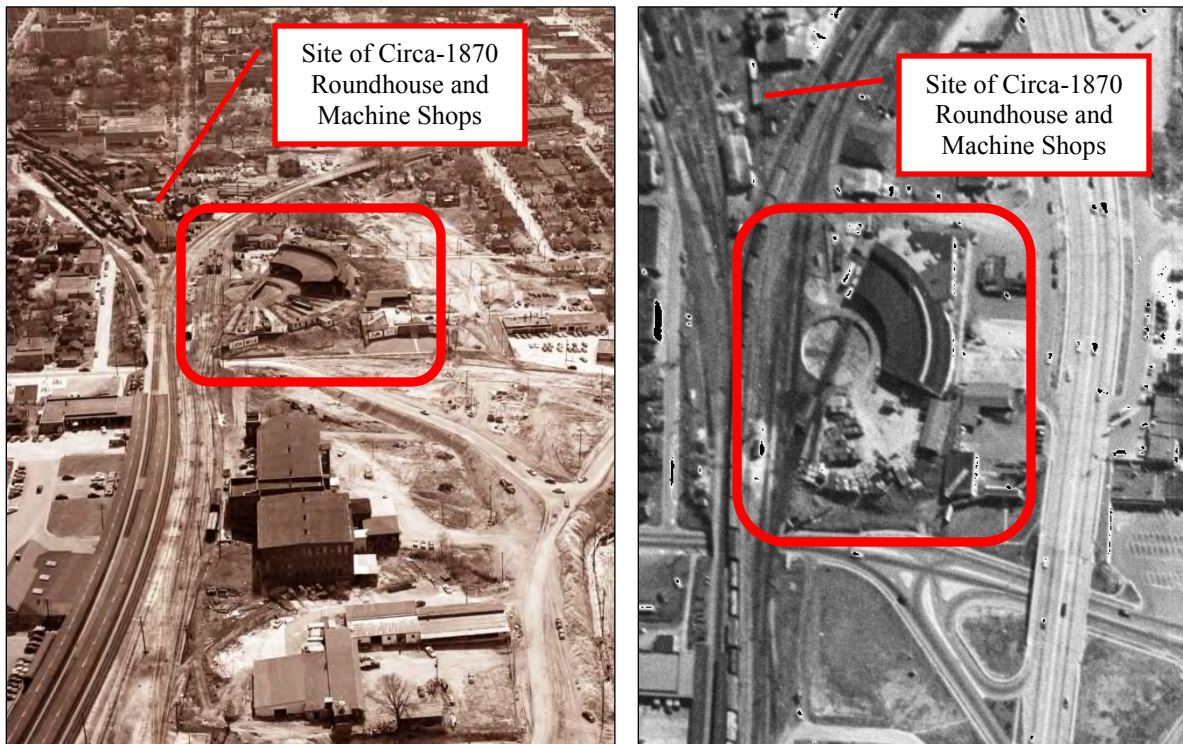


Photo 22: At Left, Detail of SAL Turntable and [Second] Roundhouse, *News and Observer* Aerial Taken During Construction of Capital Boulevard (on file, North Carolina State Archives); At Right, 1959 Aerial View of Property (USDA Historic Aerial Photos, UNC Library). The red shape highlights the resource of study.

This investigation examined the history and development of the second RGRR roundhouse and the turntable that continues to function on-site today. As the scope of this investigation does not include an archaeological assessment, the subsurface features of this property, including the remnants of the roundhouse and other associated structures, will not be addressed at length in this examination. The turntable is the only above-ground, architectural feature at the site, and is therefore, the focus of this investigation.

Physical Description

Turntables were essential to the functioning of a roundhouse, and provided methods for moving the engines in and out of the shop for cleaning, repairs, and routine maintenance. The extant turntable is believed to have been constructed in 1917 by Raleigh resident C. V. York and was needed to turn new locomotives averaging 108-feet in length, and weighing upwards of 680,000 pounds (News and Observer 1917). Historic maps indicate that this new turntable replaced an older model that sat in the very same location.

Composed primarily of riveted steel, the turntable is roughly 100-feet long and about 10-feet wide (Photo 23). The circular pit around it is constructed of poured concrete. Wood-form marks are visible on the side walls and suggest that they are an original feature. The concrete floor of the pit is sloped towards the center and may have been modified over time. Around the edge of the pit floor is a single steel track that guides the table as it turns (Photo 24, p. 44). At the time of its construction, the turntable was moved by an electrified rack and pinion system, but is today operated by a gas-power engine that is controlled from the small, gable-roofed, metal motor house (Photo 25, p. 44). Old electrical cables remain visible at the top of the structure. The wooden deck of the table is in good condition, and is likely a replacement. Steel elements of the structure are showing signs of deterioration though it remains in use.

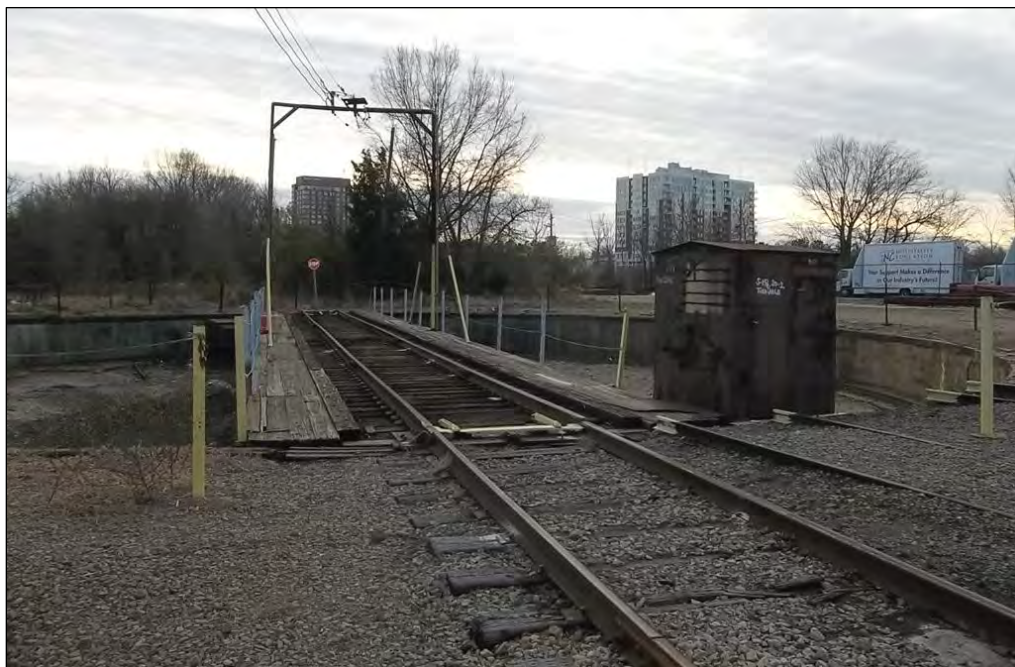


Photo 23: View of Turntable and Motor House, Looking South Southwest.



Photo 24: At Left, Turntable Pit and Track, Looking Southwest; At Right, Detail of Track.



Photo 25: “In railroad yard, water tower in foreground; shop building in background...” (Lewis P. Watson Collection, Olivia Raney Local History Library). The east end of the roundhouse, the turntable, and its motor house are visible in this circa-1940s image.

The site around the turntable is largely filled with gravel and vegetation, but remnants of old tracks, railroad ties, and various foundations are visible (Figure 15) (Photo 26, p. 46). At the northwest corner of the site, a few contemporary storage buildings are surrounded by a chain-link fence, and do not appear to be associated with the railroad today (Photo 27, p. 46). On the south side of Johnson Street are several poured-concrete foundations associated with additional railroad facilities, specifically two historic buildings labeled as offices and a restaurant. These resources, located south of the turntable, are included in the proposed NRHP boundaries of the RGRR corridor, but the adjacent roundhouse site is not highlighted as a significant component in the 2007 architectural assessment of this resource. Given the inclusion of other architectural remnants in the Johnson Street rail yard, it is likely that the omission of the half-roundhouse site from MAA's proposed NRHP boundaries was accidental.

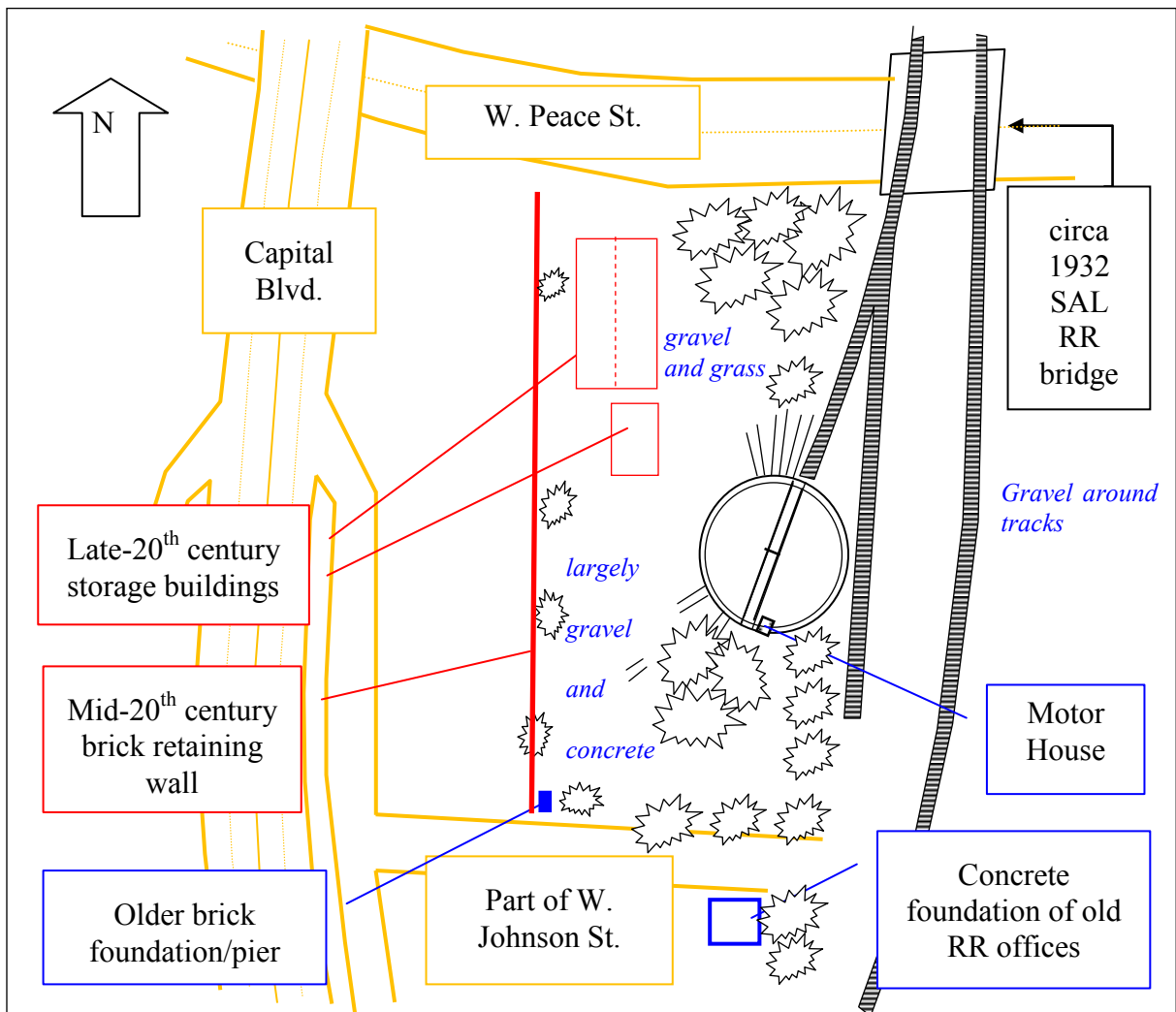


Figure 15: Site Plan of SAL Turntable and RGRR Roundhouse Site (WA 7383).



Photo 26: View of Various Materials and Foundations On-Site, Looking East.



Photo 27: View of Late-Twentieth Century Storage Buildings and Fence, Looking North from a Point Near the Southwest Corner of the Property. Note semi-circular shape of vegetation that appears to conform to the old roundhouse foundation.

NRHP Assessment

The extant turntable, railroad tracks, and surround site is historically associated with the both the Raleigh and Gaston, and Seaboard Air Line Railroad in Raleigh. The turntable structure is a component of the larger railroad corridor that continues to serve the CSX line today. As such, the turntable itself is most appropriately considered a contributing element of the NRHP-eligible RGG and SAL railroad corridor. This resource is not known to be an outstanding example of this particular type of turntable, nor is it directly associated with an important event or individual, and therefore, it is recommended not eligible as an individual resource for the NRHP under Criteria A–C. The site has been determined eligible under Criterion D (31WA1448), and should therefore, be subjected to archaeological investigation if subsurface impacts to the site are expected (Webb and Mills 1999:74) (MAA 2007:29).

Table 6: WA 7383: RGR/SAL Railroad Turntable Assessment of Historic Integrity

Aspect of Integrity	Level of Integrity	Assessment
Location	<i>High</i>	This resource remains in its original location.
Design	<i>High</i>	Architectural features remain intact and the basic design of the structure appears unaltered.
Setting	<i>Low</i>	The original setting of this resource has been repeatedly modified since initial construction, eroding quickly over last 50 years.
Materials	<i>Moderate to Low</i>	This resource appears to retain some original materials, but they are in fair to poor condition and deteriorating from exposure to the elements. Some materials, such as the wooden deck and concrete pit floor, appear to be replacements.
Workmanship	<i>Moderate to Low</i>	Some original architectural details remain intact to exhibit workmanship throughout the structure, but the poor condition of these materials has diminished the integrity of these features.
Feeling	<i>High</i>	The scale and architectural features of the structure continue to express a sense of its history.
Association	<i>Low</i>	The architecture of the turntable continues to invoke historical associations of the railroad, but the low quality of its setting has greatly diminished the site as a whole.

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APPENDIX A: BUILDING INVENTORY

B-5121 and B-5317 Building Inventory, October 2011

NC DOT#	HPO#	PROPERTY NAME	ADDRESS	NRHP ELIGIBILITY
1	WA 7355	Commercial Building; Raleigh Autos	1304 Capital Boulevard	Not Eligible
2	WA 7356	Commercial Building; Enterprise	1301 Capital Boulevard	Not Eligible
3	WA 7357	KWIX Drive In; Castle Video	1210 Capital Boulevard	Not Eligible
4	WA 7358	Esso Service Station	1200 Capital Boulevard	Not Eligible
5	WA 7359	Fairview Road Bridge over Capital Boulevard	Bridge over US 70 (Capital Boulevard)	Not Eligible
6	WA 7127	Warehouse; Consolidated Electrical Distributors (CED Tar Heel)	1125 Capital Boulevard	Determined Not Eligible by HPO Staff (May 7, 2009); Remains Not Eligible
7	WA 7126	Noland Plumbing Company	1117 Capital Boulevard	DOE by HPO Staff (Sept. 8, 2009); Additional Survey Required
8	WA 7125	Graybar Company Building; Graybar Electric Company	1113 Capital Boulevard	Determined Not Eligible by HPO Staff (May 7, 2009); Remains Not Eligible
9	WA 7360	Warehouse	110 Old Williamson Road	Not Eligible
10	WA 7007	Norfolk Southern Railroad bridge over Wade Avenue	Wade Avenue	Norfolk Southern Railway Bridges Determined Not Eligible by HPO Staff (Sept. 8, 2009); Remains Not Eligible
11	WA 7361	Bridge No. 213: Wade Avenue Bridge over Capital Boulevard	SR 1728 over SR 1511 (Capital Boulevard)	Not Eligible
12	WA 7362	Commercial Building	910 Capital Boulevard	Not Eligible
13	WA 7363	Gulf Service Station; Karl's Van Rentals	915 Capital Boulevard	Additional Survey Required
14	WA 7364	Service Station; Green Taxi	909 Capital Boulevard	Not Eligible
15	WA 7365	Commercial Building	1029 N. West Street	Not Eligible
16	WA 7366	City of Raleigh Street Maintenance Division	1027 N. West Street	Not Eligible
17	WA 7367	U-Haul Storage Facility	720 Capital Boulevard	Not Eligible

NC DOT#	HPO#	PROPERTY NAME	ADDRESS	NRHP ELIGIBILITY
18	WA 5541	Garner Commercial Building; vacant	622 Capital Boulevard	Not Eligible
19	WA 3919	Raleigh Cotton Mill; Cotton Mill Condominiums	614 Capital Boulevard	DOE by HPO Staff in 1999 and 2005; Additional Survey Required
20	WA 7368	Commercial Building	200 W Peace Street	Not Eligible
21	WA 7369	Bridge No. 227: Capital Boulevard Bridge over Peace Street	US 70 over Peace Street	Not Eligible
22	WA 7383	Seaboard Air Line Railroad Turntable and Roundhouse site	W. Johnson Street	DOE by OSA Staff in 2007; Contributing Element of NRHP-Eligible RRR and Seaboard Air Line Railroad corridor Additional Survey Required
23	WA 7370	Commercial Building	506 Capital Boulevard	Not Eligible
24	WA 7371	Commercial Building	412 Capital Boulevard	Not Eligible
25	WA 7372	Commercial Building	406 Capital Boulevard	Not Eligible
26	WA 7373	Commercial Building	404 Capital Boulevard	Not Eligible
27	WA 7374	Commercial Building	400 Capital Boulevard	Not Eligible
28	WA 7375	Commercial Building	523 Capital Boulevard	Not Eligible
29	WA 5542	Commercial Building	517 Capital Boulevard	Not Eligible
30	WA 7376	Commercial Building	312 W. Johnson Street	Not Eligible
31	WA 4591	Commercial Building; McCallum Florist Warehouse	600–620 N. West Street	Not Eligible
32	WA 7377	Commercial Building	435 W. Peace Street	Not Eligible
33	WA 3052	Economy Cleaners	407 W. Peace Street	Not Eligible
34	WA 4607	Finch's Restaurant	401 W. Peace Street	Additional Survey Required
35	WA 7378	Pigeon House Branch Culvert	W. Peace Street	Not Eligible
36	WA 7379	James H. Anderson Co.	418 W. Peace Street	Not Eligible
37	WA 7380	Commercial Building	424 W. Peace Street	Not Eligible
38	WA 7381	House; Glam Lounge	722 N. West Street	Not Eligible
39	WA 7382	Warehouse	726 N. West Street	Not Eligible
40	WA 7121	Warehouse	804 N. West Street	Determined Not Eligible by HPO Staff on May 7, 2009; Remains Not Eligible

Maps of APE Keyed to Historic Properties



Copy of Concurrence Form, November 15, 2011

Federal Aid #BRNHS-0070(119) & BRNHS-0070(149)

TIP # B-5121 & B-5317

County: Wake

CONCURRENCE FORM FOR PROPERTIES NOT ELIGIBLE FOR THE NATIONAL REGISTER OF HISTORIC PLACES

Project Description: Replace Bridge No. 227 on US 70 (Capital Blvd.) over Peace St. and Replace Bridge No. 213 on Wade Ave. over US 70 (Capital Blvd.) in Raleigh

On November 15, 2011, representatives of the

- North Carolina Department of Transportation (NCDOT)
- Federal Highway Administration (FHWA)
- North Carolina State Historic Preservation Office (NC-HPO)
- Other: Dovetail Cultural Resources Group

Reviewed the subject project at historic architectural resources photograph review session/consultation and

All parties present agreed.

- There are no properties over fifty years old within the project's Area of Potential Effects (APE).
- There are no properties less than fifty years old which are considered to meet Criteria Consideration G within the project's APE.
- There are properties over fifty years old within the project's APE, but based on the historical information available and the photographs of each property, the properties identified as 1-6, 8-12, 14-18, 20, 21, 23-33, 35-40 are considered not eligible for the National Register and no further evaluation of them is necessary. Photographs of these properties are attached.
- There are no National Register-listed or Study Listed properties within the project's APE.
- All properties greater than 50 years of age located in the APE have been considered at this consultation, and based upon the above concurrence, all compliance for historic architecture with Section 106 of the National Historic Preservation Act and GS 121-12(a) has been completed for this project.
- More information is requested on properties 7, 13, 19, 22, 34

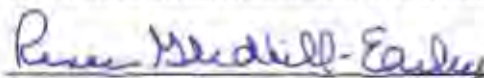
Signed:


Representative, NCDOT

11/15/2011
Date

FHWA, for the Division Administrator, or other Federal Agency

Date:


Representative, NC-HPO

11-15-11
Date

If a survey report is prepared, a final copy of this form and the attached list will be included.

Phase I Building Inventory and Evaluations

1. WA 7355: Commercial Building; Raleigh Autos
1302–1304 Capital Boulevard
PIN: 1704753695



The resource at 1302–1304 Capital Boulevard is a circa-1962, one-story, two-bay, vernacular commercial building. It appears to have been originally designed as an automobile showroom. At 1302, a stretcher-bonded brick veneer adds interest to the façade, or west elevation. The entire building is covered by a flat roof surrounded by a parapet wall with metal coping. Constructed in the mid-twentieth century, the resource remains in fair condition, but it does not exhibit high artistic value as the work of a master, nor is it an outstanding example of this particular architectural style. Moderate modifications over time have diminished its historic integrity of design, materials, workmanship, feeling and association. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

2. WA 7356: Commercial Building; Enterprise
1301 Capital Boulevard
PIN: 1704750953



The resource at 1301 Capital Boulevard is a circa-1957, one-story, one-bay, commercial building with International and vernacular styling. The north, east, and south elevations are marked by a ribbon of fixed, metal-framed, display windows which appear to be recent replacements. A flat roof surrounded by a parapet wall with metal coping covers much of the building. At the rear, or west elevation, a large, concrete-block addition is flanked by small, lean-to, frame additions clad in T1-11 siding. A covered walkway leads from the rear to a side-gabled garage and workshop, situated west of the main building.

The resource remains in fair condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of this particular architectural style. Moderate modifications over time have diminished its historic integrity of design, materials, workmanship, feeling and association. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

3. WA 7357: Commercial Building; Castle Video/KWIX Drive-In
1210 Capital Boulevard
PIN: 1704751583



The resource at 1210 Capital Boulevard is a circa-1959, one-story, flat-roof, commercial building with Modern styling. It is primarily clad in a decorative concrete block, but a large part of the façade is covered by T1-11 siding, obscuring most fenestration. Alterations to the façade appear to be a recent modification. The resource was previously occupied by a drive-in restaurant, but is currently occupied by "Castle Video and News." Though it remains in fair condition, the building does not exhibit high artistic value as the work of a master, nor is it an outstanding example of this particular architectural style. Substantial modifications have diminished its historic integrity of design, materials, workmanship, feeling and association. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

4. WA 7358: Commercial Building; Esso Service Station
1200 Capital Boulevard
PIN: 1704750421



The resource at 1200 Capital Boulevard is a circa-1958, one-story, four-bay, vernacular commercial building that was designed as a gas and service station. Although the gas pumps have been removed, the property presently functions as a service station and sales lot, operated by a company called Global Autos. The station may have originally contained three bays, as the northernmost bay appears to be a later addition. The brick veneer may have been applied to the

entire building at this time. A small lean-to addition extends from the southeast corner of the building and contains two single-leaf doors that likely lead to restrooms.

This resource was constructed in the mid-twentieth century and remains in fair condition. However, the building does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Moderate modifications over time have diminished its historic integrity of design, materials, workmanship, feeling and association. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

5. WA 7359: Fairview Road Bridge over Capital Blvd
Fairview Road
PIN: n/a (X: 2106964, Y: 745678)



The Fairview Road bridge over Capital Boulevard is a continuous, steel-stringer bridge that carries two lanes of traffic. The structure was erected in the late-1950s during the construction of Capital Boulevard. Although it remains in fair condition, this bridge is not known to be an outstanding example of this particular type of bridge, nor is it directly associated with an important event or individual. It was determined to be not eligible for the NRHP under Criteria A–C as an individual resource. It has not been evaluated under Criterion D.

6. WA 7127: Consolidated Electrical Distributors (CED); Tar Heel Electrical Supply
1125 Capital Boulevard
PIN: 1704656894



The resource at 1125 Capital Boulevard is a circa-1946, one-story, 12-bay, industrial warehouse. During the current survey it was found that this resource retains the same architectural features and historical significance detailed in previous investigations. This resource was built in the mid-

twentieth century and remains in good condition. It was previously determined to be not eligible for the NRHP as a result of “Phase I Architectural Resources Survey on the N-S Line Alternative, Southeast High Speed Rail, Raleigh, Wake County, ER03-1507” (MAA 2009a) (HPO response dated May 7, 2009), and remains not eligible for the NRHP.

8. WA 7125: Graybar Company Building
1113 Capital Boulevard
PIN: 1704653450



The Graybar Company Building at 1113 Capital Boulevard is a circa-1959, one-story, brick, Modernist warehouse and office building. During the current survey it was found that this resource retains the same architectural features and historical significance detailed in previous investigations. The resource was previously determined not eligible as a result of “Phase I Architectural Resources Survey on the N-S Line Alternative, Southeast High Speed Rail, Raleigh, Wake County, ER03-1507” (MAA 2009a) (HPO response dated May 7, 2009), and remains not eligible for the NRHP.

9. WA 7360: Commercial Building and Warehouse
110 Old Williamson Road
PIN: 1704659054



The resource at 110 Old Williamson Road is a circa-1950, office and warehouse complex. The primary resource is a one-story, three-bay, side-gabled office building, and is associated with a warehouse and other storage buildings. A covered walkway connects this addition with a long, shed-roof storage building that is also clad in T1-11 siding. This shed appears to have been constructed shortly after the primary resource. Additional non-historic storage buildings are situated on the property, and do not appear to function in relation to this resource.

The primary resource and associated storage building were constructed in the mid-twentieth century and remain in fair condition. However, the complex does not exhibit high artistic value as the work of a master, nor is it an outstanding example of this particular architectural style. Moderate modifications to the building over time have diminished its historic integrity of design, materials, workmanship, feeling and association. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

10. WA 7007: Norfolk-Southern Railroad Bridge over Wade Avenue
Wade Avenue
PIN: n/a (X: 2106829, Y: 746130)



The railroad bridge over Wade Avenue, built in the mid-1950s, is one of several similar structures associated with the Norfolk Southern line in the City of Raleigh. During the current survey, it was found that this bridge retains the same architectural features and historical significance documented in previous investigations. This resource was determined not eligible as part of the Norfolk Southern Railway Bridges examined in "Phase II Architectural Resources Survey Report for Norfolk-Southern Line in Raleigh & Areas Outside the Original Petersburg/Raleigh APE, SEHSR, P-3819, Multi-County, ER 03-1507" (MAA 2009b) (HPO response dated September 8, 2009) , and remains not eligible for the NRHP.

11. WA 7361: Bridge No. 213: Wade Avenue bridge over Capital Boulevard
Wade Avenue
PIN: n/a (X: 2106499, Y: 744778)



The Wade Avenue bridge is a circa-1954, tee-beam, reinforced-concrete bridge which carries two lanes of traffic over Capital Boulevard in the City of Raleigh (NCDOT Historic Bridge Inventory No. 910213). It was built in the mid-twentieth century during the construction of Capital Boulevard and remains in fair condition. In 2003, the bridge was surveyed during the North Carolina Historic Bridge Inventory, and recommended not eligible for the NRHP at that time. The resource is not known to be an outstanding example of this particular type of bridge, nor is it

directly associated with an important event or individual. It was determined to be not eligible for the NRHP under Criteria A–C as an individual resource. It has not been evaluated under Criterion D.

12. WA 7362: Knox Auto Sales
910 Capital Boulevard
PIN: 1704645245



The resource at 910 Capital Boulevard is a circa-1958, one-story, three-bay, commercial building with vernacular styling. The building has a continuous, concrete-block foundation and a frame structural system that is currently clad in vinyl siding. It is covered by a side-gabled roof with a front-gabled porch, supported by slender metal posts, projecting from the center of the façade, or west elevation.

Though it is in fair condition, this resource does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications over time have significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

14. WA 7364: Service Station; Green Taxi
909 Capital Boulevard
PIN: 1704641248



The resource at 909 Capital Boulevard is a circa-1955, one-story, three-bay, automobile service station with vernacular styling. Although the gas pumps have been removed, the property presently functions as a service station and sales lot, operated by the Green Taxi company. Historically covered by a flat roof, the building is presently topped by a shed roof sheathed in asphalt shingles. A one-story, three-bay, side-gabled addition was recently made to the northwest corner of the building, providing additional service space.

Though it is in fair condition, this resource does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications over time have significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

15. WA 7365: Commercial Building
1029 N. West Street
PIN: 1704537966



The resource at 1029 N. West Street is a circa-1962, one-and-a-half-story, three-bay, vernacular commercial building with several shed additions. A brick parapet facade covers a front-gabled roof, sheathed in standing-seam metal, along the east elevation. Constructed in the mid-twentieth century, the resource is in fair condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications over time have diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

16. WA 7366: City of Raleigh Street Maintenance Division Complex
1027 N. West Street (also 1014 N. West Street)
PIN: 1704528222 (X:2105992, Y: 743477)



The resource at 1014 N. West Street is a warehouse complex that appears to date from the early 1940s. The oldest section of the complex is a one-story, five-bay, vernacular warehouse, with later additions dating as late as the 1980s. The main office building is located at 1027 N. West Street. On the whole, the complex is in fair condition, but it does not exhibit high artistic value as the work of a master, nor is it an outstanding example of any particular architectural style. Substantial modifications to each of the buildings over time have diminished the historic integrity of this property. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

17. WA 7367: UHAUL Storage Facility
720 Capital Boulevard
PIN: 1704632014



The resource at 720 Capital Boulevard is a circa-1961, one-story, two-bay, warehouse that is hidden behind with vernacular styling. Hidden behind a contemporary façade is a front-gabled masonry structure with multi-light, steel-framed windows. A three-bay, shed-roof addition extends from the southeast corner of the building. Originally constructed in the mid-twentieth century, the resource is in good condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Substantial modifications to the façade and additions to the building over time have significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

18. WA 5541: Garner Building (vacant)
622 Capital Boulevard
PIN: 1704622776



The resource at 622 Capital Boulevard is a circa-1955, two-story, three-bay, Modern-era commercial building and sprawling warehouse complex. The primary massing is primarily clad in brick and covered by a flat roof, with a projecting bay at the northwest corner that is clad in an ashlar stone veneer. This section of the building appears to be vacant. A long, one-story wing extends from the northeast corner, and its north end is also clad in an ashlar stone veneer. Several additions are visible at the rear. A long, flat-roof storage building is located east of the primary resource.

Constructed in the mid-twentieth century, the resource is in fair to poor condition, and does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications to the façade and multiple additions to the building over time have significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

20. WA 7368: Commercial Building
200 W. Peace Street
PIN: 1704621184



The resource at 200 W. Peace Street is a circa-1960, one-story, six-bay, commercial building with contemporary vernacular styling. Recent modifications to the façade hide a flat-roof, Modern-era masonry structure. Although the resource is in good condition, substantial alterations to the façade have significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

21. WA 7369: Capital Boulevard bridge over W. Peace Street
Bridge No. 227 (Capital Boulevard)
PIN: n/a (X: 2105910, Y: 742051)



The Capital Boulevard Bridge over W. Peace Street is a continuous, steel-stringer bridge that carries four lanes of traffic. The structure was erected in 1953 during the construction of Capital Boulevard. This bridge was surveyed during the North Carolina Historic Bridge Inventory in 2003 and recommended not eligible for the NRHP at that time. As the resource is not known to be an outstanding example of this particular type of bridge, nor is it directly associated with an

important event or individual, it was determined to be not eligible for the NRHP under Criteria A–C as an individual resource. It has not been evaluated under Criterion D.

23. WA 7370: Commercial Building
506 Capital Boulevard
PIN: 1704610614



The resource at 506 Capital Boulevard is a circa-1959, one-story, one-bay, Modern-era commercial building. It is covered by a front-gabled roof sheathed in corrugated metal, and the façade, or west elevation, is marked by a row of fixed, display windows. Although, the resource is in fair condition, it does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications over time have diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

24. WA 7371: Commercial Building
412 Capital Boulevard
PIN: 1704610412



The resource at 412 Capital Boulevard is a circa-1955, one-story, three-bay, Modern-era commercial building with contemporary vernacular styling. It is covered by a flat roof with a double-leaf corner entry facing the intersection of W. Johnson Street and the Capital Boulevard service road. Fenestration along the west elevation has been heavily modified. Although the resource is in fair condition, it does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Substantial modifications to the façade

have significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

25. WA 7372: Commercial Building
406 Capital Boulevard
PIN: 1704610352



The resource at 406 Capital Boulevard is a circa-1960, one-story, three-bay, Modern-era commercial building with vernacular styling. The two northern storefronts have been recently replaced, while the southern bay is boarded over and appears to be vacant. The building is covered by a flat roof surrounded by a parapet wall with metal coping, and the façade is lined by a flat, metal awning supported by slender metal posts. The resource is in fair condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications to the façade have significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

26. WA 7373: Commercial Building
404 Capital Boulevard
PIN: 1704610248



The resource at 404 Capital Boulevard is a circa-1957, one-story, one-bay, Modern-era commercial building with contemporary vernacular styling. The storefront appears to be a replacement and is filled with tinted windows. The building is covered by a flat roof surrounded by a parapet wall with metal coping, and the façade is lined by a flat, metal awning supported by slender metal posts. The resource is in fair condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style.

Modifications to the façade have significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

27. WA 7374: Commercial Building
400 Capital Boulevard
PIN: 1704610222



The resource at 400 Capital Boulevard is a circa-1958, one-story, two-bay, commercial building with elements of Modern styling. The façade is angled to face the Capital Boulevard service road and anchored by a large brick massing, projecting above the roofline, at the northwest corner. The storefront is likely a replacement and a porch or awning has been removed from the west elevation. The building is covered by a flat roof surrounded by a parapet wall, and the façade is lined by a flat, metal awning supported by slender metal posts. The resource is in fair condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications to the façade have significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

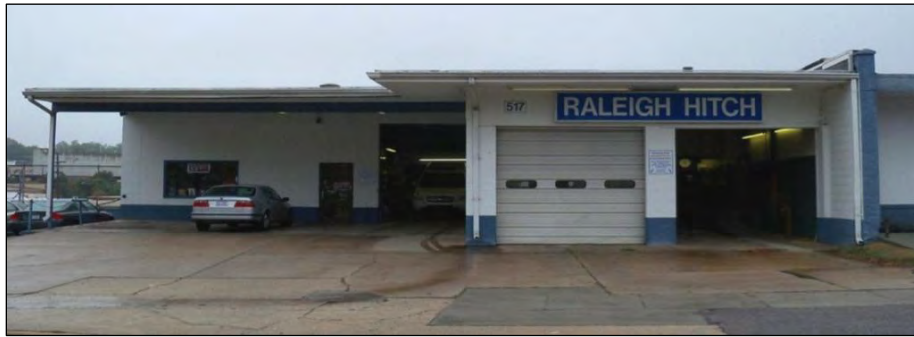
28. WA 7375: Commercial Building
523 Capital Boulevard
PIN: 1704517812



The resource at 523 Capital Boulevard is a circa-1955, two-story, three-bay, Modern-era commercial building with vernacular styling. The upper story opens onto street level with Capital Boulevard, while the lower story contains a separate business that faces west at the rear of the building. Substantial modifications have significantly diminished its historic integrity of this

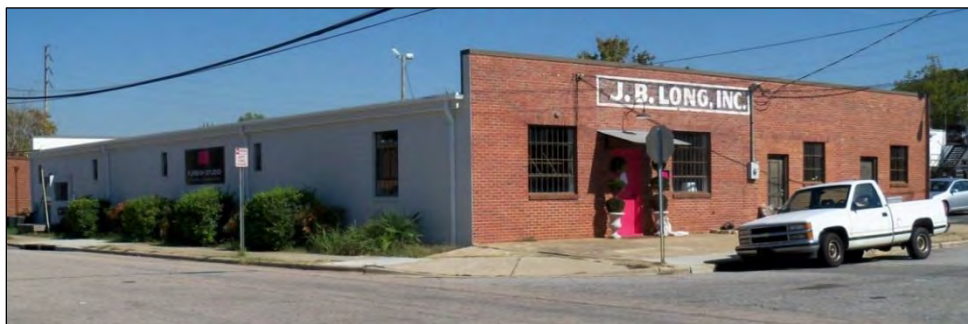
resource. It is in fair to poor condition, and does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

29. WA 5542: Commercial Building
517 Capital Boulevard
PIN: 1704516781



The resource at 517 Capital Boulevard is a circa-1956, two-story, four-bay, concrete-block commercial building with vernacular styling. It is covered by a low-pitched shed roof and directly abuts the resource at 523 Capital Boulevard. The upper story opens onto street level with Capital Boulevard, while the lower story faces west at the rear of the building. It is in fair condition, and does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Substantial modifications have diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

30. WA 7376: Commercial Building; J. B. Long, Inc. Warehouse
312 W. Johnson Street
PIN: 1704515652



The resource at 312 W. Johnson Street is a circa-1951, one-story, three-bay, warehouse and commercial building with vernacular styling. It is located at the corner of W. Johnson and N. Harrington Street. The building is covered by a low-pitched roof with brick parapet walls at both the north and south elevations. It is in fair condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Moderate modifications have diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

31. WA 4591: McCallums Florist Warehouse; Commercial Buildings
600–620 N. West Street
PIN: 1704512665



The commercial buildings at 600-620 N. West Street are located at the southeast corner of the intersection between N. West and N. Harrington Street. This group of buildings is surrounded by asphalt parking areas, driveways, and sidewalks. During the current survey, it was found that the majority of the commercial buildings on this block retain the same architectural features and historical significance documented in previous investigations (particularly 600 and 620 N. West Street). Local tax records date the property to 1957, but a portion of the building, addressed at 614 N. West Street and containing one-and-a-half stories, may be an earlier structure. The foundation at 614 is likely continuous concrete, with a concrete-block structural system that is clad in a stretcher-bonded brick veneer along the primary façade (west) and north elevations. Its front-gabled roof is sheathed in standing-seam metal with a stepped parapet wall at both the east and west elevations. Fenestration at the first floor of 614 is filled largely by contemporary, fixed, single-pane, metal-framed display windows and replacement, single-leaf metal doors. At the second floor, tripartite, multi-light, steel-framed casement windows are visible, though much of the fenestration is covered by corrugated metal. This section of the building is currently occupied by the "Southland Ballroom."

This group of commercial buildings remains in fair condition, though better examples of such warehouse and commercial buildings exist elsewhere in the City of Raleigh. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

32. WA 7377: Commercial Building; Comedy Works
435 W. Peace Street
PIN: 1704522047



The resource at 435 W. Peace Street is a circa-1960, one-story, five-bay, commercial building with vernacular styling. It is located at the corner of W. Peace and N. West Street. The building is

covered by a flat roof and clad in a brick veneer. A flat metal awning, clad in corrugated metal, lines the façade, or north elevation. The resource is in fair condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications to the façade have significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

33. WA 3052: Economy Cleaners
407 W. Peace Street
PIN: 1704516902



The resource at 407 W. Peace Street is a circa-1940, one-story, five-bay, Moderne-styled commercial building. A previous investigation noted that this resource was originally designed as a laundry, and it continues to serve this purpose today. Rising from the center of the roof is a front-gabled ridgeline that appears to have historically contained windows for ventilation. Today this area is enclosed and covered by vinyl siding. The remainder of the roof is flat and surrounded by a parapet wall with tile coping. The façade is marked by a stepped-parapet wall with metal coping. A metal awning extends across the five bays of the façade, and appears to be a later addition. A one-story outbuilding is associated with this resource. Rollins' Economy Cleaners remains in fair condition, but repeated modifications to the façade over time, particularly replacement windows, stucco cladding and awning additions, as well as the enclosure of the monitor roof, have diminished its historic integrity of design, materials, workmanship, and feeling. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

35. WA 7378: Pigeon House Branch Culvert
W. Peace Street over Pigeon House Branch
PIN: n/a (X: 2105509, Y: 742187)



The reinforced, poured-concrete culvert located on the north side of W. Peace Street channels the waters of Pigeon House Branch (also labeled Wolf Branch on some maps) under the roadway.

After its initial construction in the early- to mid-twentieth century, another bay was added to the east side of the culvert. It is presently in fair condition, but is not considered to be an outstanding example of this type of structure. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

36. WA 7379: James H. Anderson Building;
418 W. Peace Street
PIN: 1704524340



The resource at 418 W. Peace Street is a circa-1947, one-story, two-bay, warehouse with vernacular styling. It is clad in a mixture of brick and corrugated metal. A large two-story addition was made to the west side of the building, and another one-story addition abuts the rear. The building is covered by a flat roof surrounded by a parapet wall with a metal coping. The resource is in fair condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications have diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

37. WA 7380: Commercial Building
424 W. Peace Street
PIN: 1704523204



The resource at 424 W. Peace Street is a circa-1950, two-story, three-bay, concrete-block commercial building with contemporary vernacular styling. It is located at the corner of W. Peace and N. West Street, with several additions at the rear, or north elevation. The building is covered by a side-gabled roof and predominantly clad in fiberboard siding. The resource is in good condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications to the building over time have

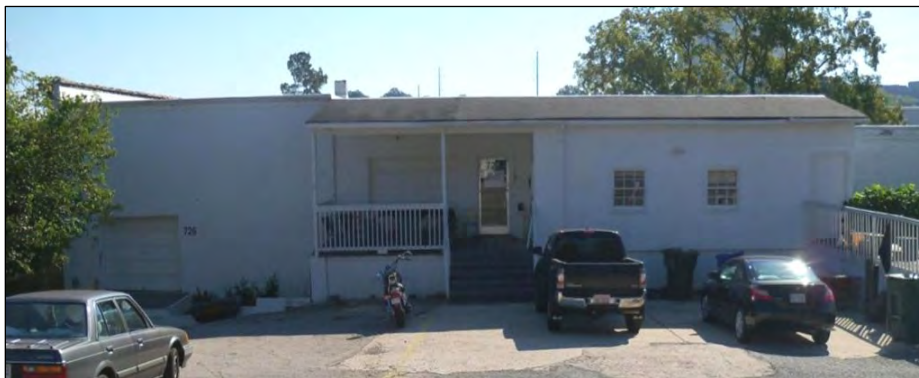
significantly diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

38. WA 7381: House; Glam Lounge
722 N. West Street
PIN: 1704523448



The resource at 722 N. West Street is a circa-1920, one-story, three-bay, residential dwelling that has been converted for commercial use. The frame dwelling is clad in vinyl siding and covered by a side-gable roof. A shed-roof porch, supported by square wood columns, extends across the façade, or west elevation. The resource is in fair condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications such as replacement windows and rear additions have diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

39. WA 7382: Warehouse
726 N. West Street
PIN: 1704524524



The resource at 726 N. West Street is a circa-1961, two-story, three-bay, concrete-block warehouse. The building is primarily covered by a flat roof. A shed-roof addition extends across the central and southern bay of the west elevation, and is open at the center bay and enclosed at the south end. The northern bay of the west façade contains an overhead, aluminum-paneled, garage door. The resource is in fair to poor condition, but does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications have diminished its historic integrity. It was determined to be not eligible for individual listing on the NRHP under Criteria A–C. It has not been evaluated under Criterion D.

40. WA 7121: Warehouse
804 N. West Street
PIN: 1704524633



The warehouse at 804 N. West Street is a circa-1930, two-story, three-bay, vernacular warehouse. During the current survey, it was found to retain the same architectural features recorded in previous investigations. The property was previously determined not eligible as a result of "Phase I Architectural Resources Survey on the N-S Line Alternative, Southeast High Speed Rail, Raleigh, Wake County, ER03-1507" (MAA 2009a) (HPO response dated May 7, 2009), and remains not eligible for the NRHP.

